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REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: _____	Name: _____
Address: _____	Address: _____
City: _____	City: _____
State: _____ ZIP: _____	State: _____ ZIP: _____
Phone: _____	Phone: _____
Email: _____	Email: _____
Contact Person: _____ Phone: _____	
Contact's Email: _____	
APPLICANT IS THE:	
<input type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input type="checkbox"/> Contract Purchaser	
Current Zoning District(s): _____ Requested Zoning District: _____	
Parcel Number(s): _____ Acreage: _____	
Property Address(es): _____	
Proposed Development: _____	
Variance(s): _____ Waiver(s): _____	
RESIDENTIAL DEVELOPMENT	NON-RESIDENTIAL DEVELOPMENT
No. of Dwelling Units: _____	No. of Buildings: _____
Dwelling Unit Sq. Ft.: _____	Total Building Sq. Ft.: _____
Density: _____	Floor Area Ratio: _____
Floor Area Ratio (LRR, MRR, HRR): _____	
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____	
Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____	

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Applicant's Letter of Intent

Havenn Homes LLC

Rezoning R-100 to RIF

The Applicant, Havenn Homes, LLC, requests to rezone a 12.154 acre parcel from R-100 to RIF (Residential Infill District) to construct a residential community with a townhome and single-family detached unit mix. The subject site is located on the south side of Russell Road and close to the west side of Buford Drive (GA-20).

The site is situated to the west of a commercial center at the intersection of Russell Road and Buford Drive (zoned C2 and OI), to the south of the Waverly Village Townhome development (zoned RTH), and to the east and north of single-family homes (zoned R100). The parcel contains a single-family home which will be demolished to make way for the proposed development.

As proposed, the community will offer a total of 105 units, resulting in a density of 8.61 units per acre. The Townhomes labeled "Autumn Leaf" on the site plan will feature front-entry garages facing internal streets. The homes labeled "Dogwood" will have rear-entry garages accessible via internal alleys; the front facade of these units will face internal streets. The "Vanderbilt" units will have rear-entry garages accessible by internal alleys, and the front façade of these units will face Russell Road. The single-family detached homes labeled "Pilot and Fathom" will have front entry garage to internal streets.

As shown on the submitted elevations, some Vanderbilt units fronting Russell Road will be 3 stories and will have exterior materials consisting of a mixture of brick and fiber cementitious siding. The remaining units will be 2 stories and will have a similar exterior composition of brick and cementitious siding. All units will feature a covered front porch and a patio.

The Vanderbilt and single-family detached units will include a two-car garage for private parking; the Dogwood and Autumn Leaf units will include a single-car garage. All will feature private driveways with additional parking space. Guest parking will be provided as parallel spaces along the interior streets. Additional site improvements will include usable open space in the form of linear parks, mews, planted areas, and sidewalks along the internal streets. Notable open space features include a dog park and gathering area with remnant fireplace.

Our plan includes a minimum 20-foot buffer along all property lines that border R100 zoning to provide space and noise reduction for adjoining single-family homes. Additionally, the single-family detached units of the proposed development will be adjacent to the neighboring R-100. As proposed the rezoning and development meets the intent of the 2045 Unified Plan. The site is located in the Ridgewood Village daily community, as part of a Low-Mix neighborhood. This neighborhood type encourages missing middle housing and easy access to the village center. Havenn Homes believes that the proposed development will serve as an ideal transition from the single-family homes in the neighboring Traditional and Traditional+ Future Development Categories to the Medium Village center. The proposed development will suitably

GWINNETT COUNTY
PLANNING AND DEVELOPMENT

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increase the density of the parcel in line with the Unified 2045 plan and provide much-needed additional housing to support Gwinnett County's growing population.

Havenn Homes looks forward to working with the Gwinnett County Planning Department and the local community to answer any questions and address any concerns. We are excited to provide highly desirable housing for the future.

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Standards Governing the Exercise of the Zoning Power

To further demonstrate that the proposed rezoning and land use is consistent with the intent of the UDO and 2045 Unified Plan, the applicant submits its response to the Standards Governing Exercise of the Zoning Power as follows:

(A) WHETHER A PROPOSED REZONING WILL PERMIT A USE THAT IS SUITABLE IN VIEW OF THE USE AND DEVELOPMENT OF ADJACENT AND NEARBY PROPERTY:

Rezoning the subject site to RIF for the purpose of constructing townhomes and single-family detached mixed development is suitable in relation to the adjacent and nearby properties. Adjacent properties are either raw land or residential use. Raw land is east of subject property and it has current zoning RTH. Residential property to south and west of property is zoned R100.

Other nearby uses include a large commercial retail center, offices, and churches.

The proposed development will add to the variety of housing in the area, and will act as a quality, low-intensity, infill use that complements the surrounding single-family uses. Further, this development will promote walkability as the area continues to attract new businesses and residents.

(B) WHETHER A PROPOSED REZONING WILL ADVERSELY AFFECT THE EXISTING USE OR USABILITY OF ADJACENT OR NEARBY PROPERTY:

The proposed rezoning will not adversely affect the use of the surrounding properties. Development standards will include buffers that will meet or exceed requirements of the category. Commercial uses nearby will benefit from new residents.

(C) WHETHER THE PROPERTY TO BE AFFECTED BY A PROPOSED REZONING HAS REASONABLE ECONOMIC USE AS CURRENTLY ZONED:

The highest and best economic use is significantly enhanced by the rezoning of the subject property. Current Zoning (R100) would allow approximately 8 single family detached lots with a net economic value (after development) of \$750,000. Property is currently under contract for more than that subject to re-zoning. The applicant submits the rezoning would allow for a use more compatible with the demand and pattern of development in the area.

(D) WHETHER THE PROPOSED REZONING WILL RESULT IN A USE WHICH WILL OR COULD CAUSE AN EXCESSIVE OR BURDENSOME USE OF EXISTING STREETS, TRANSPORTATION FACILITIES, UTILITIES, OR SCHOOLS:

The requested rezoning will not result in excessive or burdensome use of existing streets, transportation facilities, utilities, or schools. See traffic report included with this application.

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(E) WHETHER THE PROPOSED REZONING IS IN CONFORMITY WITH THE POLICY AND INTENT OF THE UNIFIED PLAN AND FUTURE DEVELOPMENT MAP:

The proposed development meets the intent of the 2045 Unified Plan. The subject property is located within the Low Mix future development category, which encourages missing middle housing. The proposed development is designed to fill the need for additional housing types in the Ridgewood Daily Community, as well as to be walkable to the village center. Further, the use serves as a transition from less dense surrounding neighborhoods to the commercial uses of the Center Medium future development category.

(F) WHETHER THERE ARE OTHER EXISTING OR CHANGING CONDITIONS AFFECTING THE USE AND DEVELOPMENT OF THE PROPERTY WHICH GIVE SUPPORTING GROUNDS FOR EITHER APPROVAL OR DISAPPROVAL OF THE PROPOSED REZONING:

The pattern of residential infill development in the area and the proposed development's compatibility with the land use of the adjacent properties are among reasons for the approval of the proposed rezoning.

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JUSTIFICATION FOR REZONING

The portions of the Gwinnett County Unified Development Ordinance (the “UDO”) which classify or may classify the property which is the subject of this Application (the “Property”) into any less intensive zoning classification other than as requested by the Applicant, are or would be unconstitutional in that they would destroy the Applicant's property rights without first paying fair, adequate and just compensation for such rights, in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

The application of the UDO as applied to the subject Property, which restricts its use to the present zoning classification, is unconstitutional, illegal, null and void, constituting a taking of the Applicant's and the Owner’s property in violation of the Just Compensation Clause of the Fifth Amendment and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States, Article I, Section I, Paragraph I, and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States denying the Applicant an economically viable use of its land while not substantially advancing legitimate state interests.

The Property is presently suitable for development under the R-IF classification as requested by the Applicant, and is not economically suitable for development under the present R-100 zoning classification of Gwinnett County. A denial of this Application would constitute an arbitrary and capricious act by the Gwinnett County Board of Commissioners without any rational basis therefore, constituting an abuse of discretion in violation of Article I, Section I, Paragraph I and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

A refusal by the Gwinnett County Board of Commissioners to rezone the Property to the R-IF classification with such conditions as agreed to by the Applicant, so as to permit the only feasible economic use of the Property, would be unconstitutional and discriminate in an arbitrary, capricious and unreasonable manner between the Applicant and owners of similarly situated property in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia

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of 1983 and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States. Any rezoning of the subject Property to the R-IF classification, subject to conditions which are different from the conditions by which the Applicant may amend its application, to the extent such different conditions would have the effect of further restricting the Applicant's and the Owner's utilization of the subject Property, would also constitute an arbitrary, capricious and discriminatory act in zoning the Property to an unconstitutional classification and would likewise violate each of the provisions of the State and Federal Constitutions set forth hereinabove.

Opponents to the request set forth in the Application, or in any amendments to the Application, have waived their rights to appeal any decision of the Gwinnett County Board of Commissioners because they lack standing, have failed to exhaust administrative remedies, and/or because they failed to assert any legal or constitutional objections.

Accordingly, the Applicant respectfully requests that the rezoning application submitted by the Applicant relative to the Property be granted and that the Property be rezoned to the zoning classification as shown on the respective application.

This 29th day of August, 2024.

Respectfully submitted,

MAHAFFEY PICKENS TUCKER, LLP

Gabrielle Schaller

Gabrielle H. Schaller
Attorneys for Applicant

1550 North Brown Road
Suite 125
Lawrenceville, Georgia 30043
(770) 232-0000

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REZONING APPLICANT'S CERTIFICATION

The undersigned below is authorized to make this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon within six (6) months from the date of last action by the Board of Commissioners.



8.22.24

Signature of Applicant

Date

MICHAEL LANGELLA

PRESIDENT

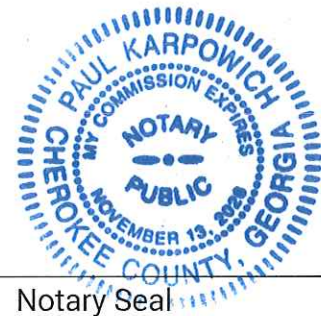
Type or Print Name and Title



8/22/24

Signature of Notary Public

Date



Notary Seal

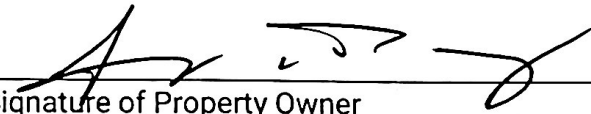
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Gwinnett County Planning Division
Rezoning Application
Last Updated 12/2023

REZONING PROPERTY OWNER'S CERTIFICATION

The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon within six (6) months from the date of last action by the Board of Commissioners.



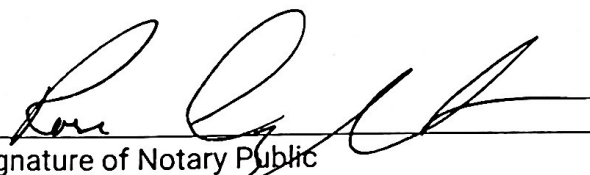
Signature of Property Owner

8/20/24

Date

Gregory W Peavy, owner

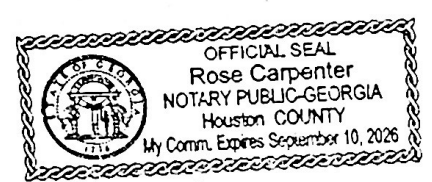
Type or Print Name and Title



Signature of Notary Public

8/20/24

Date




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CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

 9/9/24 Gregory W. Peavy, Owner
Signature of Applicant Date Type of Print Name and Title

Signature of Applicant's Date Type or Print Name and Title
Attorney or Representative

 9/9/24 Sheila Hill
Signature of Notary Public Date NOTARY PUBLIC
Houston County, GEORGIA
My Commission Expires 07/19/2025

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

Yes No Gregory Peavy (Your Name)

If the answer is yes, please complete the following section:

NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL	CONTRIBUTIONS (List all which aggregate to \$250 or More)	DATE CONTRIBUTION WAS MADE (Within last two years)

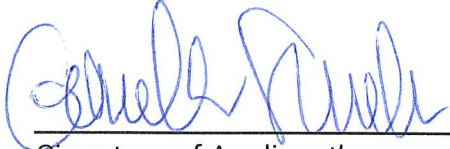
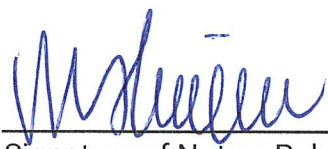
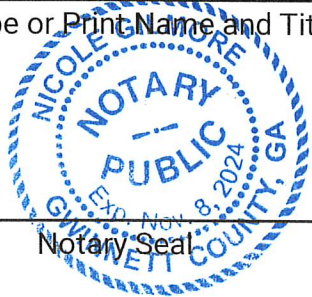
Attach additional sheets if necessary to disclose or describe all contributions.

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CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

Signature of Applicant	Date	Type of Print Name and Title
	8/29/2024	Gabrielle Schaller, attorney for Applicant
Signature of Applicant's Attorney or Representative	Date	Type or Print Name and Title
	8/29/2024	
Signature of Notary Public	Date	Notary Seal
		

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

Yes No Gabrielle Schaller, attorney for Applicant (Your Name)

If the answer is yes, please complete the following section:

NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL	CONTRIBUTIONS (List all which aggregate to \$250 or More)	DATE CONTRIBUTION WAS MADE (Within last two years)
Please see attached		

Attach additional sheets if necessary to disclose or describe all contributions.

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**CONFLICT OF INTEREST CERTIFICATION FOR REZONING/CHANGE IN
CONDITIONS AND SPECIAL USE PERMIT APPLICATIONS**

MAHAFFEY PICKENS TUCKER, LLP

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTIONS</u>	<u>DATE</u>
Ben Ku, Commissioner	\$1,500	10/03/2022
Matt Holtkamp, Commissioner	\$1,000	04/13/2023
Nicole Love Hendrickson, Chairwoman	\$2,500	08/11/2023
Matt Holtkamp, Commissioner	\$3,300	09/18/2023
Kirkland Carden, Commissioner	\$3,300	09/27/2023

Lee Tucker, Attorney at Law, P.C.

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTIONS</u>	<u>DATE</u>
Kirkland Carden, Commissioner	\$2,000	10/25/2023

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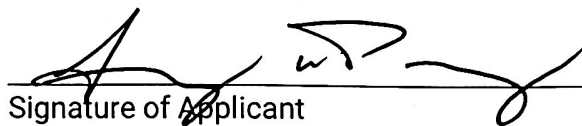
8.29.2024

VERIFICATION OF CURRENT PAID PROPERTY TAXES FOR REZONING

The undersigned below is authorized to make this application. The undersigned certifies that all Gwinnett County property taxes billed to date for the parcel listed below have been paid in full to the Gwinnett County Tax Commissioner. In no case shall an application or reapplication for rezoning be processed without such property verification.

A SEPARATE VERIFICATION FORM MUST BE COMPLETED FOR EACH TAX PARCEL INCLUDED IN THE REZONING REQUEST.

Parcel I.D. Number: B7066 002
(Map Reference Number)


Signature of Applicant

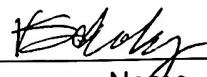
8/20/24
Date

Gregory W Peavy, owner
Type or Print Name and Title

PLEASE TAKE THIS FORM TO THE TAX COMMISSIONERS OFFICE AT THE GWINNETT JUSTICE AND ADMINISTRATION CENTER, 75 LANGLEY DRIVE. THIS FORM MUST BE SIGNED BY A REPRESENTATIVE OF THE TAX COMMISSIONER'S OFFICE.

TAX COMMISSIONERS USE ONLY

Payment of all property taxes billed to date for the above referenced parcel has been verified as paid current and confirmed by the signature below.


Name

TSA - II
Title

8/20/2024
Date

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Legal Description

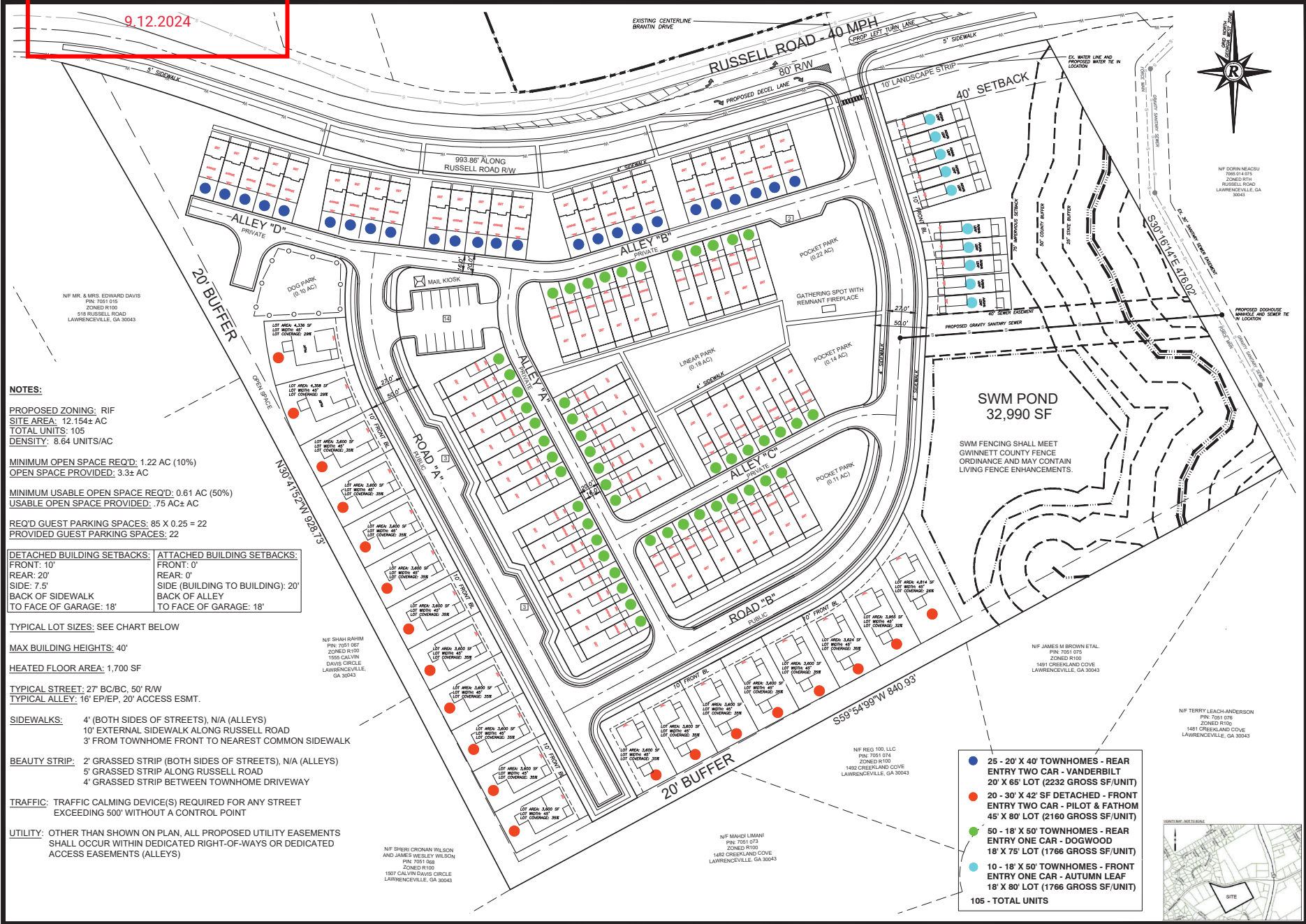
All that tract and parcel of land lying and being in Land Lot 66 of the 7th District, Gwinnett County, Georgia, and being more particularly described as follows:

Beginning at the Northeasterly corner of Land Lot 66, where said Land Lot intersects and meets at the corners of Land Lot 65, Land Lot 52 and Land Lot 51, beginning at said corner where an 18 inch Elm forms the corner of said Land Lots, and thence running in a Southwesterly direction, South 59 degrees 54 minutes 99 seconds West a distance of 840.93 feet to an iron pin; thence running in a Northwesterly direction North 30 degrees 41 minutes 52 seconds West a distance of 928.73 feet to an iron pin, located on the Southerly side of the right-of-way of Russell Road, said right-of-way being 80 feet in width; thence following in an Easterly and Northerly direction, the curvature of said right-of-way a distance of 993.86 feet to a point where the right-of-way intersect with the Northern land lot line of Land Lot 66, at a point where an iron pin is located; thence running in a Southeasterly direction South 30 degrees 16 minutes 14 seconds East a distance of 476.02 feet to the original point of beginning.

Said property being improved and containing 12.154 acres of land, according to the survey of said property prepared by Randall W. Dixon, Registered Land Surveyor, of Precision Planning, Inc., according to a plat dated July 29, 1985.

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- NOTES:**
- PROPOSED ZONING: RIF
 - SITE AREA: 12.154± AC
 - TOTAL UNITS: 105
 - DENSITY: 8.64 UNITS/AC
 - MINIMUM OPEN SPACE REQ'D: 1.22 AC (10%)
 - OPEN SPACE PROVIDED: 3.3± AC
 - MINIMUM USABLE OPEN SPACE REQ'D: 0.61 AC (50%)
 - USABLE OPEN SPACE PROVIDED: .75 AC± AC
 - REQ'D GUEST PARKING SPACES: 85 X 0.25 = 22
 - PROVIDED GUEST PARKING SPACES: 22
- | | |
|---|--------------------------------------|
| DETACHED BUILDING SETBACKS: | ATTACHED BUILDING SETBACKS: |
| FRONT: 10' | FRONT: 0' |
| REAR: 20' | REAR: 0' |
| SIDE: 7.5' | SIDE (BUILDING TO BUILDING): 20' |
| BACK OF SIDEWALK TO FACE OF GARAGE: 18' | BACK OF ALLEY TO FACE OF GARAGE: 18' |
- TYPICAL LOT SIZES: SEE CHART BELOW
 - MAX BUILDING HEIGHTS: 40'
 - HEATED FLOOR AREA: 1,700 SF
 - TYPICAL STREET: 27' BC/BC, 50' R/W
 - TYPICAL ALLEY: 16' EP/EP, 20' ACCESS ESMT.
 - SIDEWALKS:**
 - 4' (BOTH SIDES OF STREETS) N/A (ALLEYS)
 - 10' EXTERNAL SIDEWALK ALONG RUSSELL ROAD
 - 3' FROM TOWNHOME FRONT TO NEAREST COMMON SIDEWALK
 - BEAUTY STRIP:**
 - 2' GRASSED STRIP (BOTH SIDES OF STREETS), N/A (ALLEYS)
 - 5' GRASSED STRIP ALONG RUSSELL ROAD
 - 4' GRASSED STRIP BETWEEN TOWNHOME DRIVEWAY
 - TRAFFIC:** TRAFFIC CALMING DEVICE(S) REQUIRED FOR ANY STREET EXCEEDING 500' WITHOUT A CONTROL POINT
 - UTILITY:** OTHER THAN SHOWN ON PLAN, ALL PROPOSED UTILITY EASEMENTS SHALL OCCUR WITHIN DEDICATED RIGHT-OF-WAYS OR DEDICATED ACCESS EASEMENTS (ALLEYS)

- 25 - 20' X 40' TOWNHOMES - REAR ENTRY TWO CAR - VANDERBILT 20' X 65' LOT (2232 GROSS SF/UNIT)
- 20 - 30' X 42' SF DETACHED - FRONT ENTRY TWO CAR - PILOT & FATHOM 45' X 80' LOT (2160 GROSS SF/UNIT)
- 50 - 18' X 50' TOWNHOMES - REAR ENTRY ONE CAR - DOGWOOD 18' X 75' LOT (1766 GROSS SF/UNIT)
- 10 - 18' X 50' TOWNHOMES - FRONT ENTRY ONE CAR - AUTUMN LEAF 18' X 80' LOT (1766 GROSS SF/UNIT)
- 105 - TOTAL UNITS

NF DORIN HEACUS
706-614-875
20865 WINDYBUSH
LAWRENCEVILLE, GA 30043

NF SHAH RAHIM
PIN: 705 1 027
ZONED R100
1505 CALVIN
DAVIS CIRCLE
LAWRENCEVILLE,
GA 30043

NF JAMES M BROWN ETAL
PIN: 705 978
ZONED R100
1481 GREENLAND COVE
LAWRENCEVILLE, GA 30043

NF TERRY LEACH-ANDERSON
PIN: 705 076
ZONED R100
1481 GREENLAND COVE
LAWRENCEVILLE, GA 30043

NF MAURICE LIMANI
PIN: 705 873
ZONED R100
1482 GREENLAND COVE
LAWRENCEVILLE, GA 30043

NF REG 100, LLC
PIN: 705 074
ZONED R100
1492 GREENLAND COVE
LAWRENCEVILLE, GA 30043

NF SHERI CRONAN WILSON
AND JAMES WESLEY WILSON
PIN: 705 158
ZONED R100
1507 CALVIN DAVIS CIRCLE
LAWRENCEVILLE, GA 30043

SWM POND
32,990 SF

SWM FENCING SHALL MEET
GWINNETT COUNTY FENCE
ORDINANCE AND MAY CONTAIN
LIVING FENCE ENHANCEMENTS.

PROPOSED CONDUIT FOR
LANDSCAPE STRIP
IN LOCATION

PROPOSED GRASSY SANITARY SEWER
IN LOCATION

PROPOSED 48" SEWER EASEMENT
IN LOCATION

PROPOSED 48" WATER LINE AND
PROPOSED WATER TIE IN
LOCATION

PROPOSED 48" GAS LINE AND
PROPOSED GAS TIE IN
LOCATION

PROPOSED 48" CABLE TV
AND PROPOSED CABLE TV
TIE IN LOCATION

PROPOSED 48" FIBER OPTIC
AND PROPOSED FIBER OPTIC
TIE IN LOCATION

PROPOSED 48" POWER
AND PROPOSED POWER
TIE IN LOCATION

PROPOSED 48" TELEPHONE
AND PROPOSED TELEPHONE
TIE IN LOCATION

PROPOSED 48" CABLE TV
AND PROPOSED CABLE TV
TIE IN LOCATION

PROPOSED 48" FIBER OPTIC
AND PROPOSED FIBER OPTIC
TIE IN LOCATION

PROPOSED 48" POWER
AND PROPOSED POWER
TIE IN LOCATION

PROPOSED 48" TELEPHONE
AND PROPOSED TELEPHONE
TIE IN LOCATION

GRAPHIC SCALE

0' 20' 40' 60'

DATE: 09/10/24
SCALE: 1" = 40'-0" (AS SHOWN)
SHEET NO.: 022060-005
DATE: 09/10/24
DRAWN BY: CMW / JAS

Rochester **DECM**

Rochester & Associates, LLC
425 Oak St NW, Gainesville, GA 30601
770.716.8000 | www.rochester-decm.com

ZONING PLAN FOR
SUMMER HAVEN
588 RUSSELL ROAD TRACT
LAND LOT 66, 77A, DISTRICT
GWINNETT COUNTY, GEORGIA

NO.	DATE	DESCRIPTION	REVISIONS

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ELEVATION C



ELEVATION D



ELEVATION E

Fathom

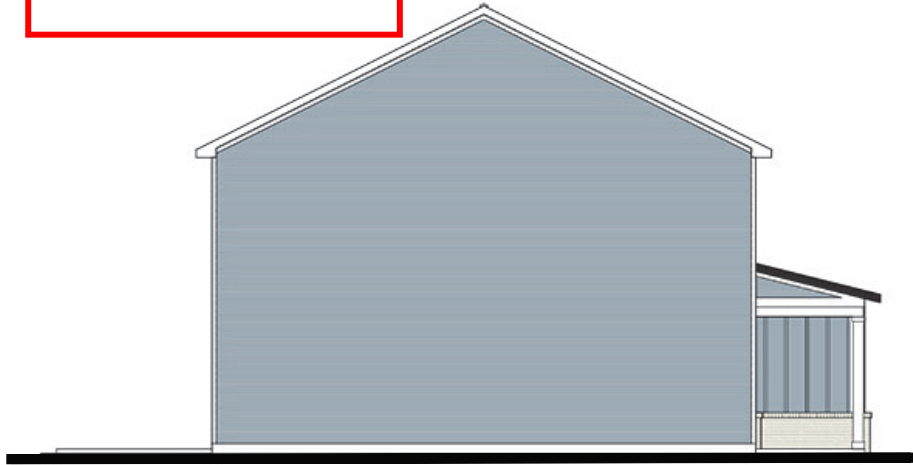
Front Elevations



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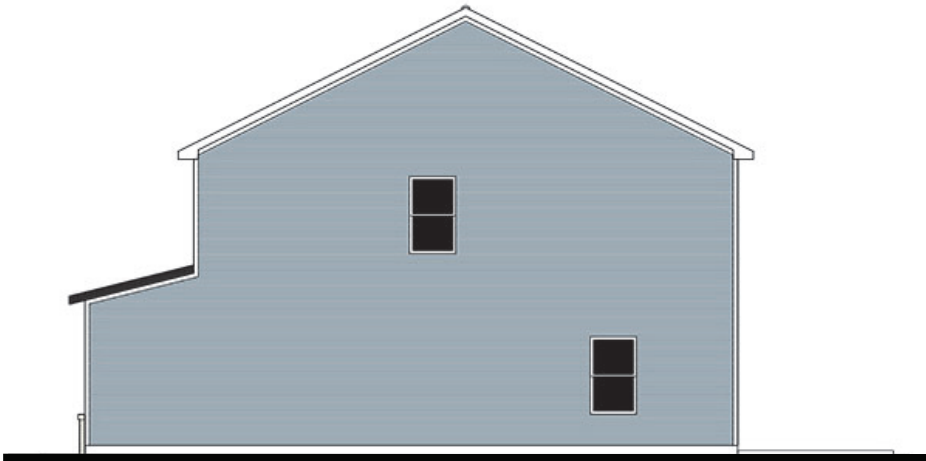
8.29.2024



2 LEFT ELEVATION "E"
SCALE 1/8"=1'-0"



1 FRONT ELEVATION "E"
SCALE 1/8"=1'-0"



4 RIGHT ELEVATION "E"
SCALE 1/8"=1'-0"



2 REAR ELEVATION "E"
SCALE 1/8"=1'-0"

AH²
DESIGN GROUP, LLC
Architectural Planning
Professional Services
1000 Peachtree Street, NE, Suite 400
Atlanta, GA 30309
www.ah2design.com

PROJECT: 29'-9" WIDE SINGLE FAMILY

SCALE: 1/8"=1'-0"

DATE: 8/29/24

NO.	ISSUES & REVISIONS	DATE	BY

NO.	ISSUES & REVISIONS	DATE	BY

PROJECT: PILOT
29'-9" WIDE SINGLE FAMILY

CONTENTS:
ELEVATION "E"

DATE: 8/29/24
PAGE NO:

SHEET NO:
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AH²
 DESIGN GROUP, LLC
 200 Peachtree Parkway
 Roswell, GA 30076
 770-585-5233
 www.ah2design.com

FIVE ORACLE PARKWAY
 ROSSVILLE, GA 30088

SEAL:

I warrant that these plans do not violate any applicable laws, codes, ordinances or regulations of any jurisdiction. I warrant that these plans are true and correct to the best of my knowledge and belief.

NO.	ISSUES & REVISIONS	DATE	BY

PROJECT:
**PILOT
 29'-9\"/>**

CONTENTS:
 ELEVATION "C"

DATE: _____ PAGE NO: _____

SHEET NO:
A-201

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DOGWOOD
ELEVATION A

DOGWOOD
ELEVATION C

DOGWOOD
ELEVATION D

1 RUSSELL ROAD DOGWOOD BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0" 3 UNIT BUILDING



PRINT RECORD

#	DATE	DESCRIPTION
1	08.20.24	PERMIT
2		
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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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SCALE: 1/8"=1'-0" ON 11x17 SHEET

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**BUILDING
ELEVATIONS**

Drawn By:

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DOGWOOD
ELEVATION D

DOGWOOD
ELEVATION G

DOGWOOD
ELEVATION A

1 RUSSELL ROAD DOGWOOD BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0" 3 UNIT BUILDING



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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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August 20, 2024

SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE
BUILDING
ELEVATIONS

Drawn By

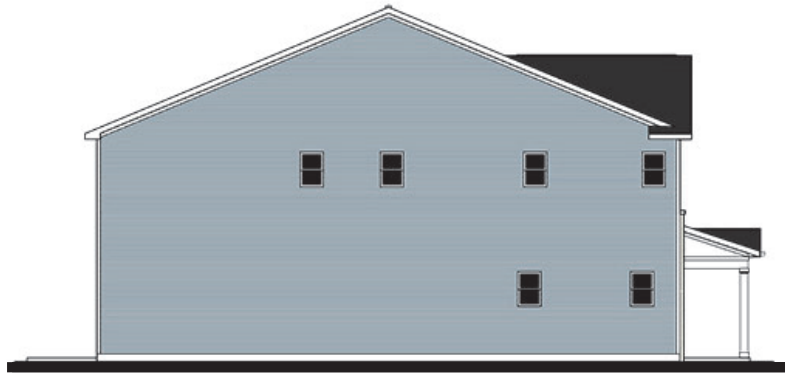
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DOGWOOD
ELEVATION A

1 RUSSELL ROAD DOGWOOD BUILDING - LEFT ELEVATION
SCALE 3/8"=1'-0" 3 UNIT BUILDING



DOGWOOD
ELEVATION D

2 RUSSELL ROAD DOGWOOD BUILDING - RIGHT ELEVATION
SCALE 3/8"=1'-0" 3 UNIT BUILDING



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**RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING**

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1 RUSSELL ROAD 18' TOWNHOME BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0" 5 UNIT BUILDING



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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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1 RUSSELL ROAD 18' TOWNHOME BUILDING - REAR ELEVATION
SCALE: 1/4"=1'-0" 5 UNIT BUILDING



PRINT RECORD

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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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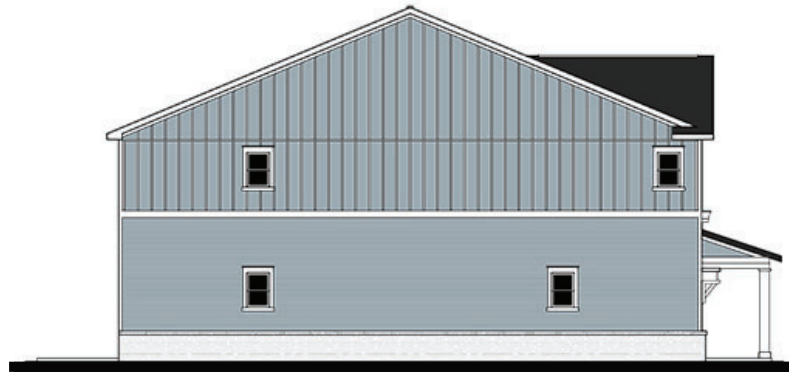
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BUILDING ELEVATIONS

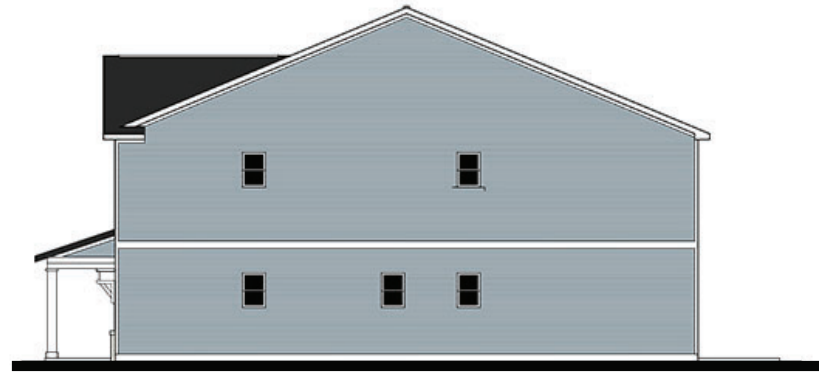
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Checked By:
Project# A-202

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1 LEFT ELEVATION "A"
SCALE 3/8"=1'-0"



2 RIGHT ELEVATION "E"
SCALE 3/8"=1'-0"



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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#	DATE	DESCRIPTION
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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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UNIT #1
ELEVATION "A"

UNIT #2
ELEVATION "B"

UNIT #3
ELEVATION "D"

UNIT #4
ELEVATION "E"

UNIT #5
ELEVATION "C"

1 RUSSELL ROAD VANDERBILT BUILDING - FRONT ELEVATION
SCALE: 1/4"=1'-0"
5 UNIT BUILDING



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HOMES**
1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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August 20, 2024

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ELEVATIONS**

Drawn By:
Checked By: **A-201**
Project#:

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UNIT #5
ELEVATION 'C'

UNIT #4
ELEVATION 'E'

UNIT #3
ELEVATION 'D'

UNIT #2
ELEVATION 'B'

UNIT #1
ELEVATION 'A'

1 RUSSELL ROAD VANDERBILT BUILDING - REAR ELEVATION
SCALE: 1/4"=1'-0"
5 UNIT BUILDING



**HAVENN
HOMES**
1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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#	DATE	DESCRIPTION
1	08.20.24	PERMIT
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RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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SCALE:

RELEASE DATE:

August 20, 2024

SCALE: 1/8"=1'-0" ON 11x17 SHEET

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**BUILDING
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**HAVENN
HOMES**
1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

PRINT RECORD

#	DATE	DESCRIPTION
1	08.20.24	PERMIT
2		
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4		

RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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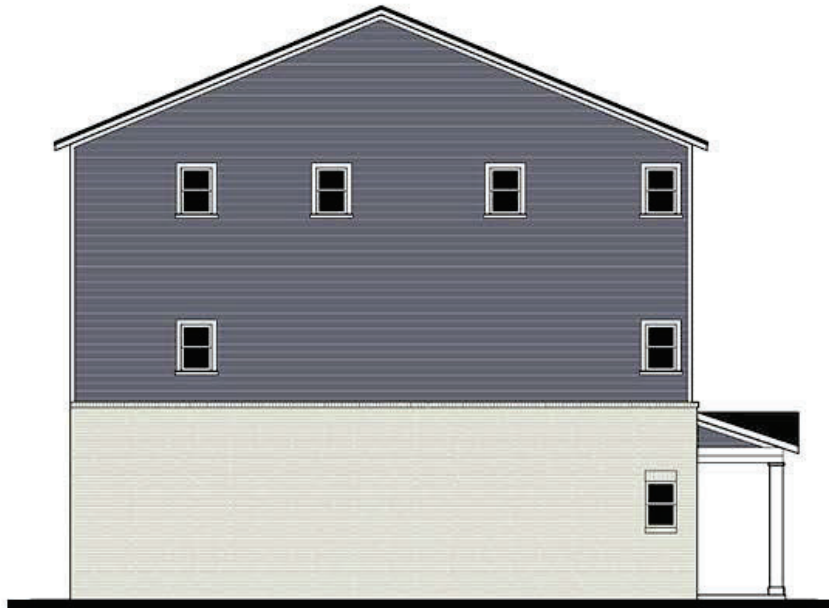
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ISSUE DATE
August 20, 2024

SCALE: 1/8"=1'-0" ON 11x17 SHEET

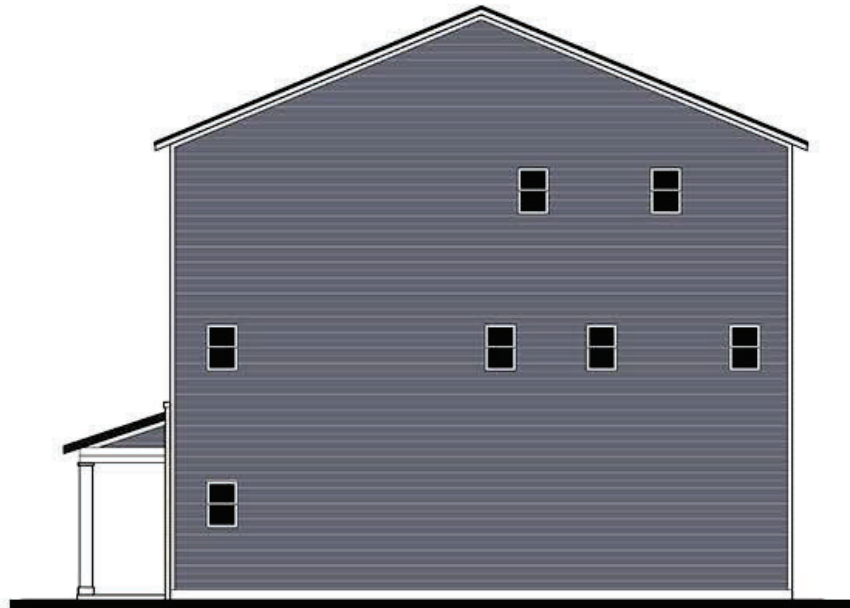
DRAWING TITLE
**BUILDING
ELEVATIONS**

Drawn By
Checked By
Project# **A-203**



UNIT #1
ELEVATION "A"

1 LEFT ELEVATION "A"
SCALE 1/4"=1'-0"



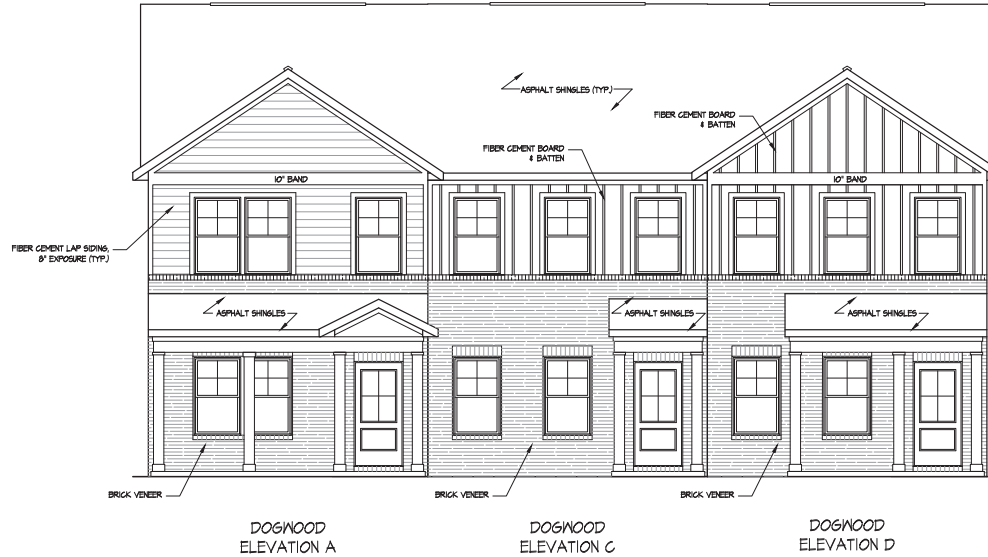
UNIT #5
ELEVATION "C"

2 RIGHT ELEVATION "C"
SCALE 1/4"=1'-0"

RECEIVED

9.12.2024

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.



1 **RUSSELL ROAD DOGWOOD BUILDING - FRONT ELEVATION**
SCALE 1/4"=1'-0"
3 UNIT BUILDING

MATERIAL HATCHING TABLE	
	FIBER CEMENT LAP SIDING 8" EXPOSURE (TYP)
	FIBER CEMENT BOARD AND BATTEN
	BRICK VENEER
	ASPHALT SHINGLES
ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL.	
ALL ROOFING TO BE ASPHALT SHINGLE.	



PRINT RECORD

#	DATE	DESCRIPTION
1	08.20.24	PERMIT
2		
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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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August 20, 2024

SCALE: 1/8"=1'-0" ON 11x17 SHEET

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**BUILDING
ELEVATIONS**

Drawn By
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9.12.2024



1 RUSSELL ROAD DOGWOOD BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0" 3 UNIT BUILDING

MATERIAL HATCHING TABLE	
	FIBER CEMENT LAP SIDING 8\"/>
	FIBER CEMENT LAP SIDING 8\"/>
	FIBER CEMENT BOARD AND BATTS
	ALL BRICK, BRICK PATTERN, BRICK COLOR TO BE PROVIDED FROM CLIENT MATERIAL
	ALL ROOFING TO BE ASPHALT SHINGLE



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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BUILDING
ELEVATIONS

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Checked By
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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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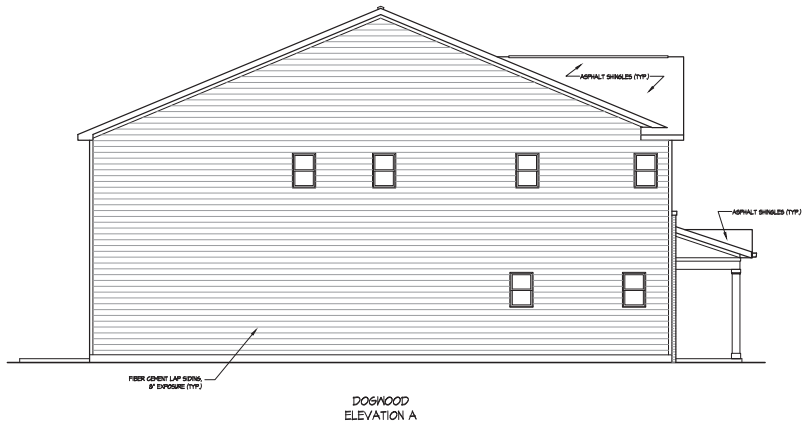
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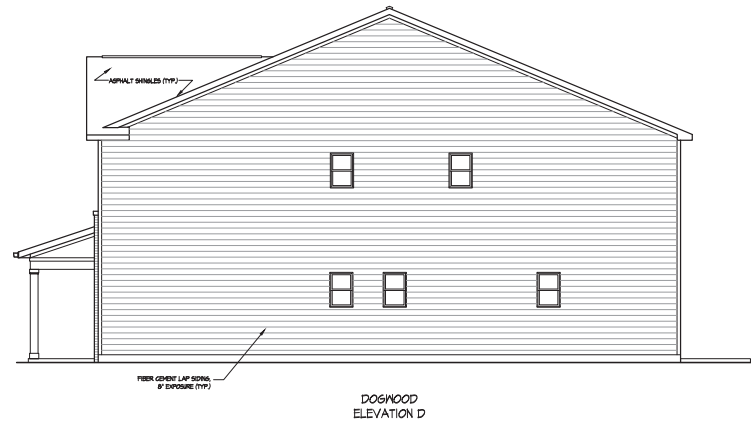
DRAWING TITLE
BUILDING
ELEVATIONS

Drawn By
Checked By
Project#
A-203



DOGWOOD
ELEVATION A

1 RUSSELL ROAD DOGWOOD BUILDING - LEFT ELEVATION
SCALE 3/16"=1'-0" 3 UNIT BUILDING



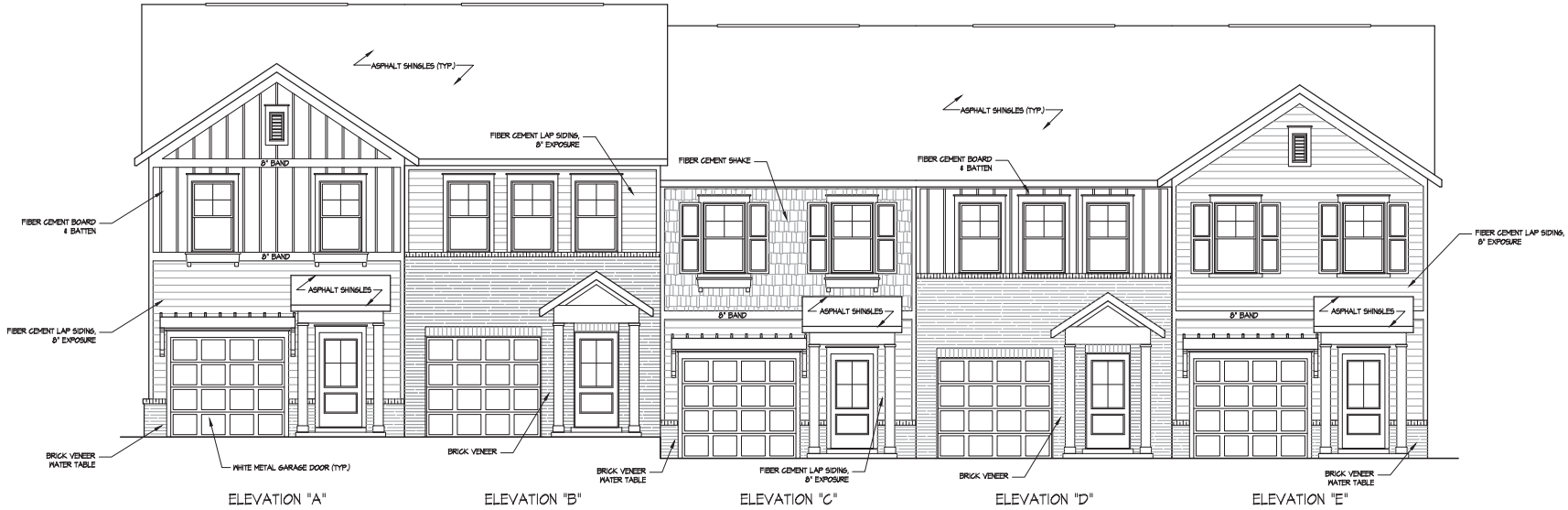
DOGWOOD
ELEVATION D

2 RUSSELL ROAD DOGWOOD BUILDING - RIGHT ELEVATION
SCALE 3/16"=1'-0" 3 UNIT BUILDING

MATERIAL HATCHING TABLE	
	FREE COSENT LAP SIDING @ EXP. TYP.
	FREE COSENT LAP SIDING @ EXP. TYP.
	FREE COSENT BOARD AND BATTEN
	ALL TEXT, SYMBOLS, PICTORIAL SYMBOLS, COLORS TO BE PRINTED FREE COSENT MATERIAL
	ALL ROOFING TO BE ASPHALT SHINGLE

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9.12.2024



1 RUSSELL ROAD 18' TOWNHOME BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.

MATERIAL HATCHING TABLE	
	FIBER CEMENT LAP SIDING 8' EXP. TYP.
	FIBER CEMENT LAP SIDING 6' EXP. TYP.
	FIBER CEMENT BOARD 4 BATTEN
	BRICK VENEER WATER TABLE
	ASPHALT SHINGLES (TYP.)
	WHITE METAL GARAGE DOOR (TYP.)



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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BUILDING
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Project#

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1 RUSSELL ROAD 18' TOWNHOME BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0" 5 UNIT BUILDING

MATERIAL HATCHING TABLE	
	FIBER CEMENT LAP SIDING 8" EXP. TYP.
	FIBER CEMENT LAP SIDING 8" EXP. TYP.
	FIBER CEMENT BOARD AND BATTEN
	ALL FIBER CEMENT PANELS SHALL BE PAINTED PER LATEST PRACTICE.
	ALL ROOFS TO BE ASPHALT SHINGLE



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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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**BUILDING
ELEVATIONS**

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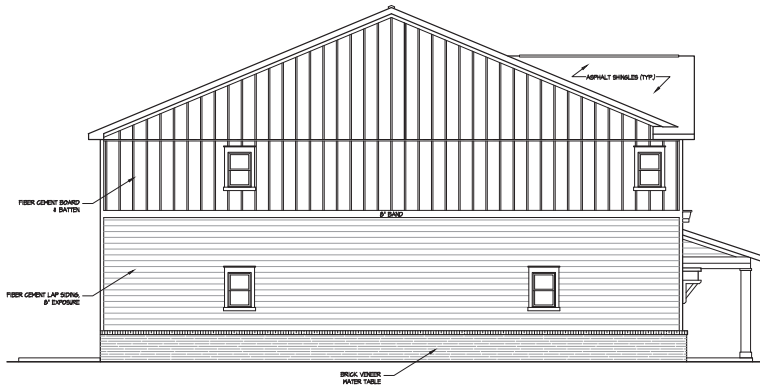
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1 LEFT ELEVATION "A"
SCALE 3/16"=1'-0"



2 RIGHT ELEVATION "E"
SCALE 3/16"=1'-0"

MATERIAL HATCHING TABLE	
	FIBER CEMENT LAP SIDING 8" EXPOSURE (TYP.)
	FIBER CEMENT LAP SIDING 8" EXPOSURE (TYP.)
	FIBER CEMENT BOARD AND BATON
	ALL TEN SQUARE BRICK VENEER COLLARS TO BE FINISHED WITH CONCRETE MASONRY
	ALL ROOFING TO BE ASPHALT SHINGLE



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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Drawn By

Checked By

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1 RUSSELL ROAD VANDERBILT BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

MATERIAL HATCHING TABLE

	-FIBER CEMENT LAP SIDING 8' EXP. TYP.
	-FIBER CEMENT LAP SIDING 9' EXP. TYP.
	-FIBER CEMENT BOARD AND BATTEN
	-ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL
	-ALL ROOFING TO BE ASPHALT SHINGLE



PRINT RECORD

#	DATE	DESCRIPTION
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RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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BUILDING
ELEVATIONS

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1 RUSSELL ROAD VANDERBILT BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING

MATERIAL HATCHING TABLE	
	-FIBER CEMENT LAP SIDING 8" EXP. TYP.
	-FIBER CEMENT LAP SIDING 9" EXP. TYP.
	-FIBER CEMENT BOARD AND BATTEN
	-ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL
	-ALL ROOFING TO BE ASPHALT SHINGLE



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RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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BUILDING ELEVATIONS

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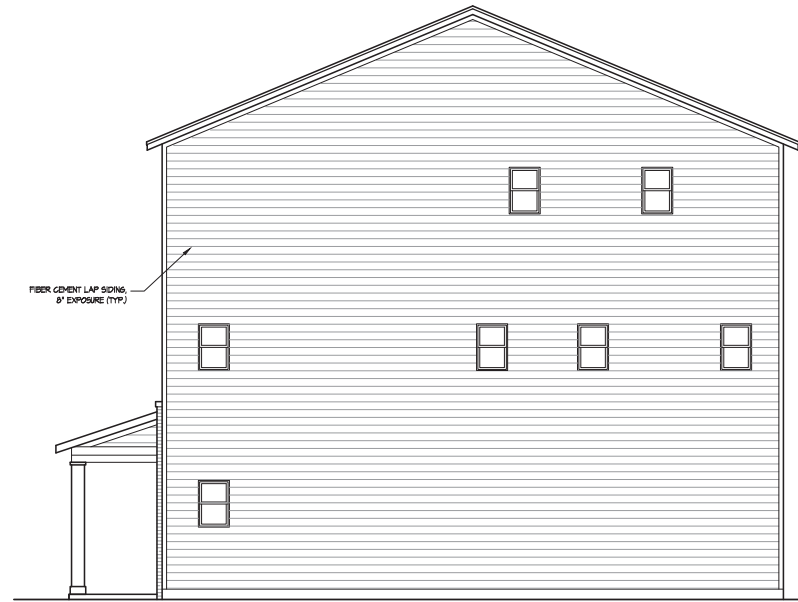
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UNIT #1
ELEVATION "A"

1 LEFT ELEVATION "A"
SCALE 1/4"=1'-0"



UNIT #5
ELEVATION "C"

2 RIGHT ELEVATION "C"
SCALE 1/4"=1'-0"

MATERIAL HATCHING TABLE	
	-FIBER CEMENT LAP SIDING 8" EXP. TYP.
	-FIBER CEMENT LAP SIDING 8" EXP. TYP.
	-FIBER CEMENT BOARD AND BATTEN
	-ALL TRIM, SOFFIT, FASCIA, EAVES, GABLES TO BE PAINTED FIBER CEMENT MATERIAL.
	-ALL ROOFING TO BE ASPHALT SHINGLE



PRINT RECORD

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RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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SCALE: 1/8"=1'-0" ON 11x17 SHEET

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**BUILDING
ELEVATIONS**

Drawn By

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Project#

A-203

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AH²
DESIGN GROUP, LLC
Architecture/Planning
1100 CIRCLE 25 THUNDERBOLT
SUITE 270
ATLANTA, GA 30320
www.ah2designgroup.com



SEAL:

Quantity of issues raised and resolved
ALL COMMENTS AND REVISIONS MUST BE ACCOMPANIED BY A COVER SHEET WITH THE FOLLOWING INFORMATION:
DATE: 8/1/2024
NO. ISSUES & REVISIONS: 1
DATE: 8/1/2024
NO. ISSUES & REVISIONS: 1
PERMIT SET

NO.	ISSUES & REVISIONS	DATE
1		
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PROJECT:
FATHOM ELEVATION "C"
26' WIDE SINGLE FAMILY

CONTENTS:
ELEVATION "C"

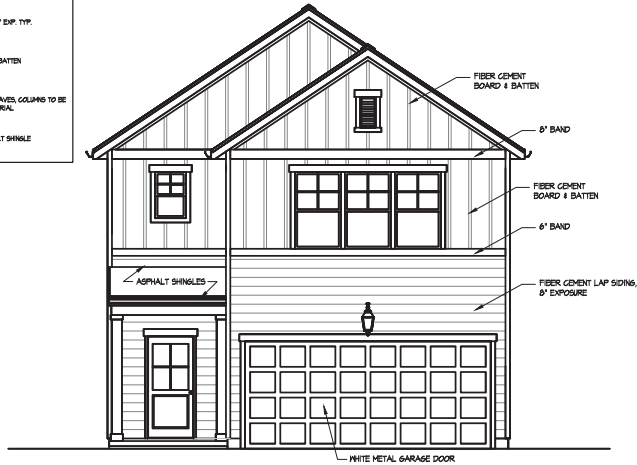
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SHEET NO:

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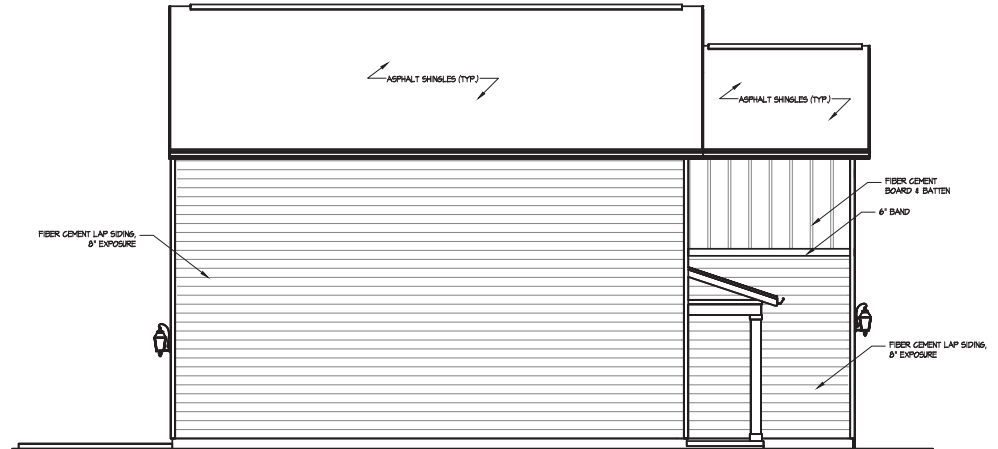
MATERIAL HATCHING TABLE

	-FIBER CEMENT LAP SIDING 8' EXP. TYP.
	-FIBER CEMENT LAP SIDING 8' EXP. TYP.
	-FIBER CEMENT BOARD AND BATTEN
	-ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL
	-ALL ROOFING TO BE ASPHALT SHINGLE



1 FRONT ELEVATION "C"
SCALE 1/8"=1'-0"

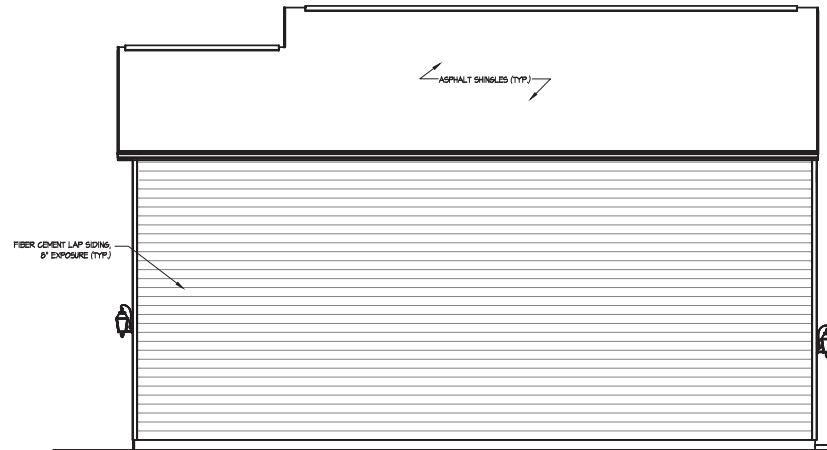
ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL



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SCALE 1/8"=1'-0"



3 REAR ELEVATION "C"
SCALE 1/8"=1'-0"



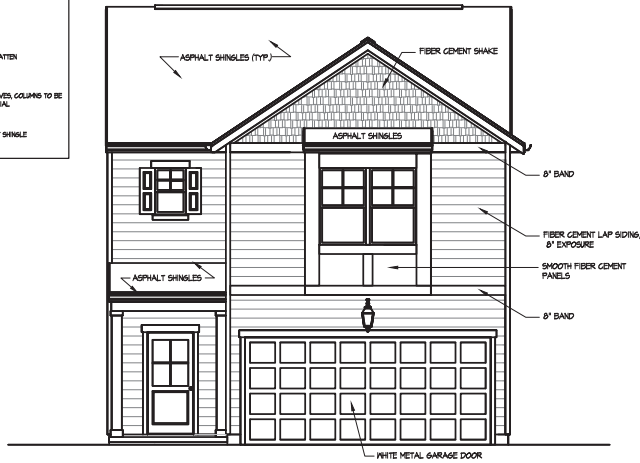
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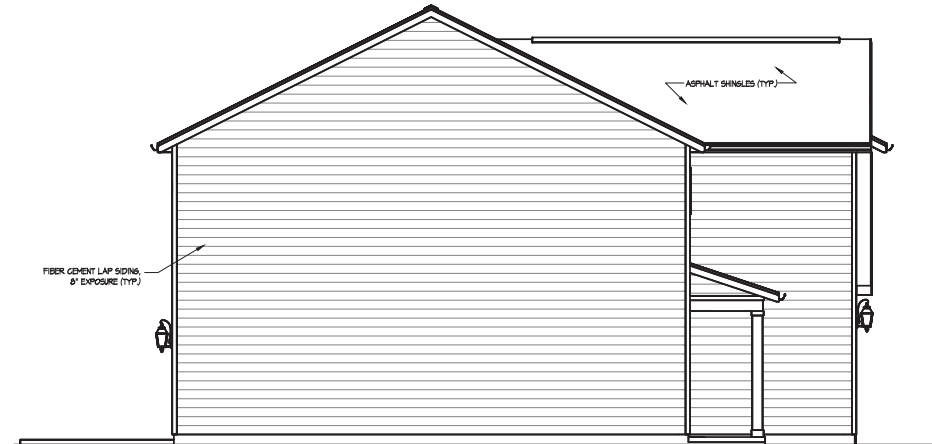
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	-ALL ROOFING TO BE ASPHALT SHINGLE



1 FRONT ELEVATION "D"
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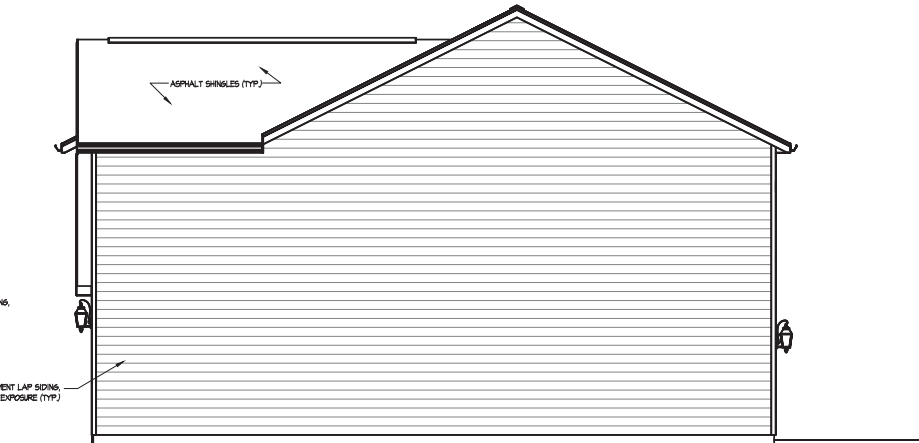
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COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.



2 LEFT ELEVATION "D"
SCALE 1/8"=1'-0"



3 REAR ELEVATION "D"
SCALE 1/8"=1'-0"



4 RIGHT ELEVATION "D"
SCALE 1/8"=1'-0"

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1100 CIRCLE 15 PARKWAY
SUITE 270
ATLANTA, GA 30309

SEAL:

DATE: 09/12/24
NO. ISSUES & REVISIONS: 1

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PROJECT: FATHOM ELEVATION "D"
26' WIDE SINGLE FAMILY

CONTENTS:
ELEVATION "D"

DATE: 09/12/24

PROJ. NO.:

SHEET NO.:

A-201

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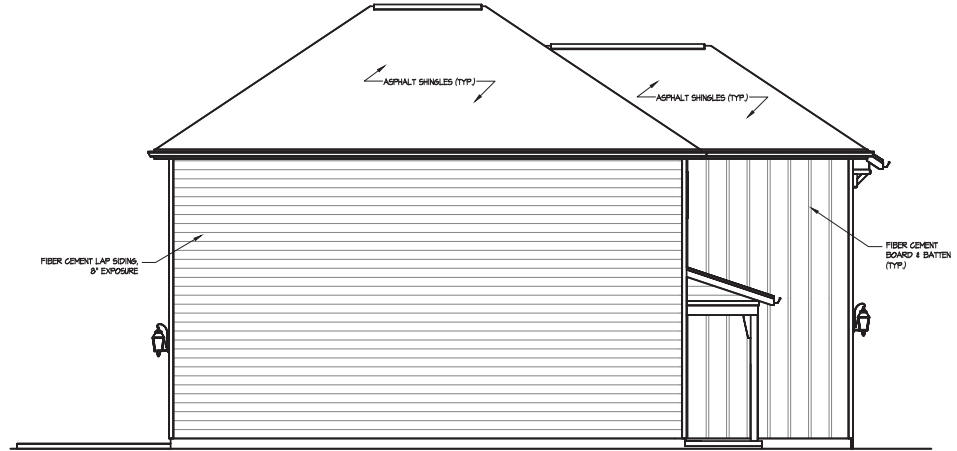
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	-ALL ROOFING TO BE ASPHALT SHINGLE



1 FRONT ELEVATION "E"
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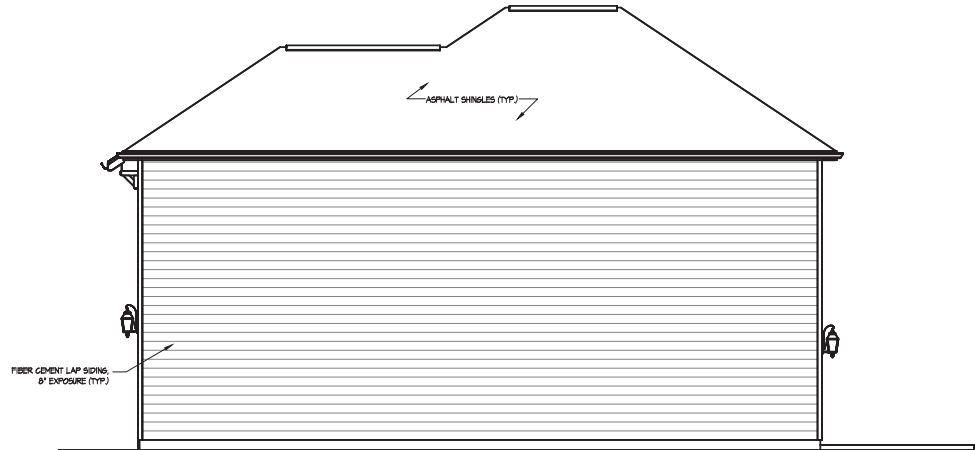
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



2 LEFT ELEVATION "E"
SCALE 1/4"=1'-0"



3 REAR ELEVATION "E"
SCALE 1/4"=1'-0"



4 RIGHT ELEVATION "E"
SCALE 1/4"=1'-0"

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1100 CIRCLE 25, FARMERSBUR
SUITE 270
ATLANTA, GA 30320

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DATE: 08/12/2024

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DATE: 08/12/2024

PROJECT: FATHOM ELEVATION "E"
26' WIDE SINGLE FAMILY

CONTENTS:
ELEVATION "E"

DATE: 08/12/2024

PROJ. NO.

SHEET NO:

A-201

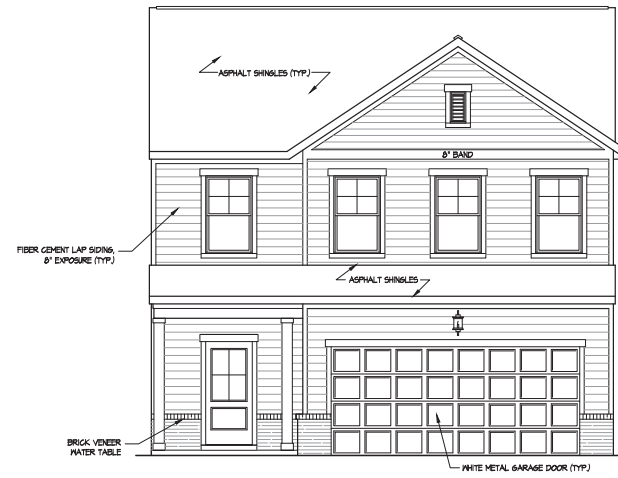
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ALL TRIM, SOFFIT, FASCIA, EAVES,
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1 FRONT ELEVATION 'C'
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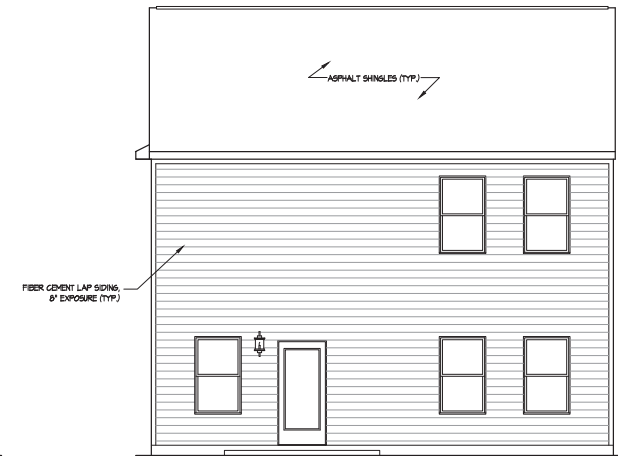
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	FIBER CEMENT BOARD AND BATTEN
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4 RIGHT ELEVATION 'C'
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3 REAR ELEVATION 'C'
SCALE 1/4"=1'-0"

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COLUMNS TO BE PAINTED FIBER
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Architecture/Planning
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SUITE 270
ATLANTA, GA 30320



SEAL:

QUALITY OF WORK AND SERVICE OF ARCHITECTURE/PLANNING IS GUARANTEED AND NOT SUBJECT TO ANY OTHER CONTRACT DOCUMENTS OR CONDITIONS OF CONTRACT. THE ARCHITECT'S LIABILITY IS LIMITED TO THE CONTRACT AMOUNT.

NO.	ISSUES & REVISIONS	DATE
1	PERMIT SET	8/21/2024

PROJECT:
PILOT
29'-9" WIDE SINGLE FAMILY

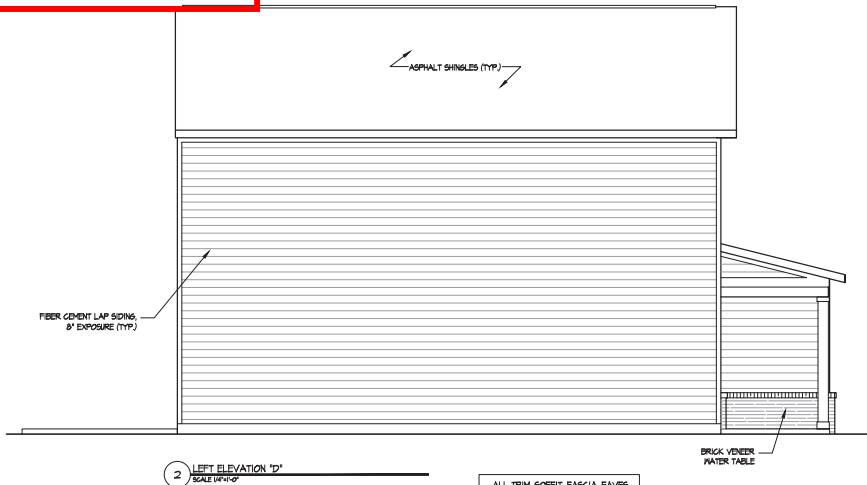
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ELEVATION 'C'

DATE: PROJ. NO.

SHEET NO:
A-201

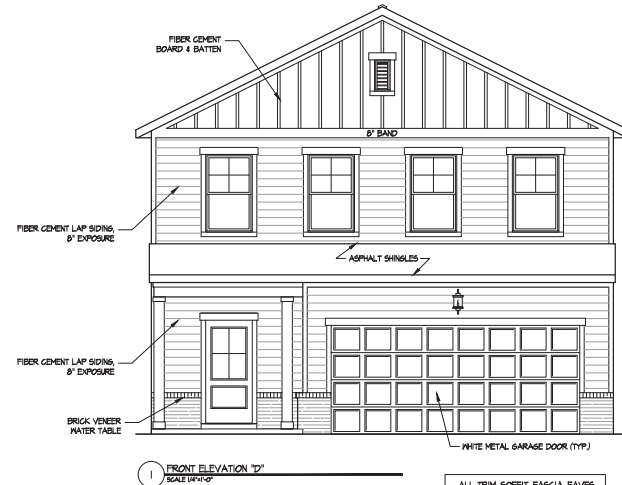
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3 LEFT ELEVATION 'D'
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ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.

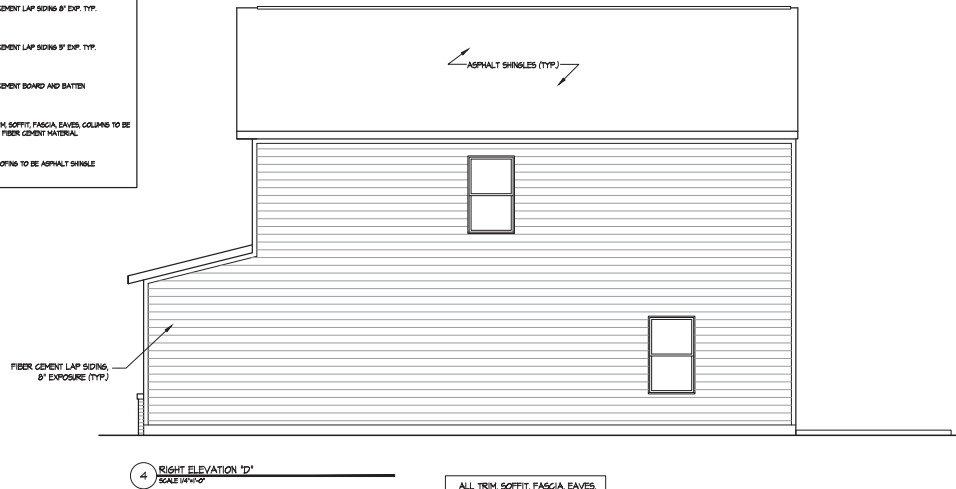


1 FRONT ELEVATION 'D'
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.

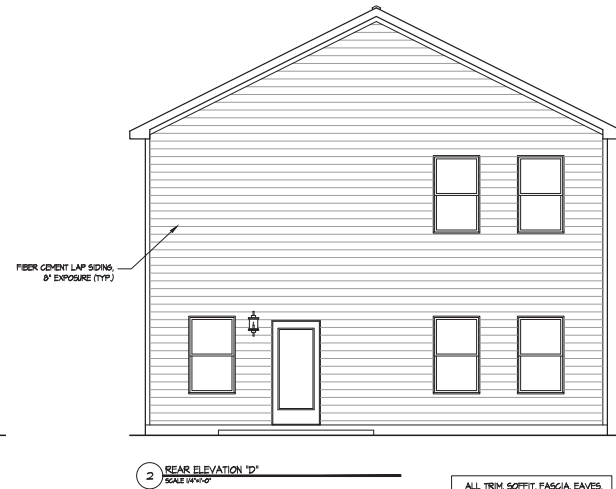
MATERIAL HATCHING TABLE

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	FIBER CEMENT BOARD AND BATTEN
	ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL.
	ALL ROOFING TO BE ASPHALT SHINGLE



4 RIGHT ELEVATION 'D'
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
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2 REAR ELEVATION 'D'
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.

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Atlanta, GA 30309



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30309

SEAL:

DATE: 8/2/2024

NO. ISSUES & REVISIONS

DATE: 8/2/2024

NO. ISSUES & REVISIONS

DATE: 8/2/2024

NO. ISSUES & REVISIONS

DATE: 8/2/2024

NO. ISSUES & REVISIONS

DATE: 8/2/2024

PROJECT: PILOT

29'-9" WIDE SINGLE FAMILY

CONTENTS:

ELEVATION 'D'

DATE: 8/2/2024

SHEET NO:

A-203

Public Participation Plan

1. **Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?**

2. **Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.**

3. **What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.**

4. **What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?**



September 11, 2024

Jonathan Love
Havenn Homes, LLC
1100 Circle 75 Pkwy, Suite 270
Atlanta, GA 30339

<input type="checkbox"/>	APPROVED
<input type="checkbox"/>	DENIED
<input checked="" type="checkbox"/>	CONDITIONALLY APPROVED
Sewer Capacity Request #C2024-224-09	
Expiration Date: 09/11/2025	
Tie-In Manhole FID: 207495	

RE: Sewer Availability for Proposed Development – Summer Haven – Russell Road Tract
Parcel ID 7066 002

Dear Mr. Love:

This letter supersedes the earlier Sewer Capacity Certification C2021-01-011 dated January 14, 2021 addressed to Brian Taylor of Taylor Morrison.

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of 85 townhomes and 20 single-family homes on the above parcel and determined:

County can provide sewer service for the proposed development only upon completion of the following conditions:

- Completion by Gwinnett County of the Ridge Road Pump Station Decommissioning project with approximate completion date of 2026
- Coordination on the schedule, phasing, and design of the referenced development in relation to GCDWR’s capacity expansion project in the area and the anticipated sewer connection

This confirmation is based on your anticipated annual average daily flow of **18.23 gpm** discharging to the sewer tie-in manhole at Facility ID **207495**.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.





Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

Lorraine Campagne, PE
Section Manager, Development Support
678.376.7058

C: Gwinnett County DWR – Della Taylor, Division Director; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Geniva Sylvain, Engineer I; Christina Dejarnette, Engineer I;
Wendell Tumale Engineer III

GWINNETT COUNTY
PLANNING AND DEVELOPMENT

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**TRAFFIC IMPACT STUDY
FOR
PROPOSED RESIDENTIAL DEVELOPMENT
AT 598 RUSSELL ROAD**

GWINNETT COUNTY, GEORGIA



Prepared for:

***Havenn Homes
1100 Circle 75 Parkway, Suite 270
Havenn Homes, GA, 30339***

Prepared By:



A&R Engineering Inc.

2160 Kingston Court, Suite O
Marietta, GA 30067
Tel: (770) 690-9255 Fax: (770) 690-9210
www.areneg.com

August 07, 2024
A & R Project # 24-147

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1.0 INTRODUCTION

The purpose of this study is to determine the traffic impact from the proposed residential development that will be located at 598 Russell Road in Gwinnett County, Georgia. The traffic analysis evaluates the current operations and future conditions with the traffic generated by the development. The development will consist of 112 townhomes and proposes one full access driveway on Russell Road.



The AM, school dismissal, and PM peak hours have been analysed in this study. This study includes the evaluation of traffic operations at the intersections of:

1. SR 20 (Buford Drive) at Russell Road / Ridge Road
2. Russell Road at Brantin Drive
3. Russell Road at Calvin Davis Circle

Recommendations to improve traffic operations have been identified as appropriate and are discussed in detail in the following sections of the report. The location of the development and the surrounding roadway network are shown in Figure 1.

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Study Intersection



LOCATION MAP

FIGURE 1

A&R Engineering Inc.

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2.0 EXISTING FACILITIES / CONDITIONS

2.1 Roadway Facilities

The following is a brief description of each of the roadway facilities located in proximity to the site:

2.1.1 SR 20 (Buford Drive)

SR 20 (Buford Drive) is a north-south, four-lane, median-divided roadway with a posted speed limit of 50 mph in the vicinity of the site. Georgia Department of Transportation (GDOT) traffic counts (Station ID: 135-0125) indicate that the daily traffic volume on SR 20 (Buford Drive) in 2023 was 34,500 vehicles per day north of Russell Road. Both GDOT and Gwinnett County classify SR 20 (Buford Drive) as a principal arterial roadway.

2.1.2 Russell Road

Russell Road is an east-west, two-lane, undivided roadway with a posted speed limit of 40 mph in the vicinity of the site. Georgia Department of Transportation (GDOT) traffic counts (Station ID: 135-8025) indicate that the daily traffic volume on Russell Road in 2023 was 8,220 vehicles per day west of Calvin Davis Drive. Russell Road changes names to the Ridge Road to the east of its intersection with SR 20. Gwinnett County classifies Russell Road as a major collector roadway.

2.1.3 Ridge Road

Ridge Road is an east-west, two-lane, undivided roadway with a posted speed limit of 40 mph in the vicinity of the site. Ridge Road changes names to Russell Road to the west of its intersection with SR 20.

2.1.4 Calvin Davis Circle

Calvin Davis Circle is a two-lane undivided roadway with a posted speed limit of 25 mph in the vicinity of the site.

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3.0 STUDY METHODOLOGY

In this study, the methodology used for evaluating traffic operations at each of the subject intersections is based on the criteria set forth in the Transportation Research Board Highway Capacity Manual, 6th edition (HCM 6). Synchro software, which utilizes the HCM methodology, was used for the analysis. The following is a description of the methodology employed for the analysis of unsignalized and signalized intersections.

3.1 Unsignalized Intersections

For unsignalized intersections controlled by a stop sign on minor streets, the level of service (LOS) for motor vehicles with controlled movements is determined by the computed control delay according to the thresholds stated in Table 1 below. LOS is determined for each minor street movement (or shared movement), as well as major street left turns. LOS is not defined for the intersection as a whole or for major street approaches. The LOS of any controlled movement which experiences a volume-to-capacity ratio greater than 1 is designated as “F” regardless of the control delay.

Control delay for unsignalized intersections includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Several factors affect the control delay for unsignalized intersections, such as the availability and distribution of gaps in the conflicting traffic stream, critical gaps, and follow-up time for a vehicle in the queue.

Level of service is assigned a letter designation from “A” through “F”. Level of service “A” indicates excellent operations with little delay to motorists, while level of service “F” exists when there are insufficient gaps of acceptable size to allow vehicles on the side street to cross the main road without experiencing long delays.

TABLE 1 — LEVEL-OF-SERVICE CRITERIA FOR UNSIGNALIZED INTERSECTIONS		
Control Delay (sec/vehicle)	LOS by Volume-to-Capacity Ratio*	
	v/c ≤ 1.0	v/c > 1.0
≤ 10	A	F
> 10 and ≤ 15	B	F
> 15 and ≤ 25	C	F
> 25 and ≤ 35	D	F
> 35 and ≤ 50	E	F
> 50	F	F

*The LOS criteria apply to each lane on a given approach and to each approach on the minor street. LOS is not calculated for major-street approaches or for the intersection.

Source: Highway Capacity Manual, 6th edition, Exhibit 20-2 *LOS Criteria: Motorized Vehicle Mode*

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3.2 Signalized Intersections

According to HCM procedures, LOS can be calculated for the entire intersection, each intersection approach, and each lane group. HCM uses control delay alone to characterize LOS for the entire intersection or an approach. Control delay per vehicle is composed of initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Both control delay and volume-to-capacity ratio are used to characterize LOS for a lane group. A volume-to-capacity ratio greater than 1.0 for a lane group indicates failure from capacity perspective. Therefore, such a lane group is assigned LOS F regardless of the amount of control delay.

Table 2 below summarizes the LOS criteria from HCM for motorized vehicles at signalized intersection.

TABLE 2 – LEVEL-OF-SERVICE CRITERIA FOR SIGNALIZED INTERSECTIONS		
Control Delay (sec/vehicle) *	LOS for Lane Group by Volume-to-Capacity Ratio*	
	v/c ≤ 1.0	v/c > 1.0
≤ 10	A	F
> 10 and ≤ 20	B	F
> 20 and ≤ 35	C	F
> 35 and ≤ 55	D	F
> 55 and ≤ 80	E	F
> 80	F	F

*For approach-based and intersection wide assessments, LOS is defined solely by control delay

Source: Highway Capacity Manual, 6th edition, Exhibit 19-8 *LOS Criteria: Motorized Vehicle Mode*

LOS A is typically assigned when the volume-to-capacity (v/c) ratio is low and either progression is exceptionally favourable, or the cycle length is very short. LOS B is typically assigned when the v/c ratio is low and either progression is highly favorable, or the cycle length is short. However, more vehicles are stopped than with LOS A. LOS C is typically assigned when progression is favorable, or the cycle length is moderate. Individual *cycle failures* (one or more queued vehicles are not able to depart because of insufficient capacity during the cycle) may begin to appear at this level. Many vehicles still pass through the intersection without stopping, but the number of vehicles stopping is significant. LOS D is typically assigned when the v/c ratio is high and either progression is ineffective, or the cycle length is long. There are many vehicle-stops and individual cycle failures are noticeable. LOS E is typically assigned when the v/c ratio is high, progression is very poor, the cycle length is long, and individual cycle failures are frequent. LOS F is typically assigned when the v/c ratio is very high, progression is very poor, the cycle length is long, and most cycles fail to clear the queue.

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4.0 ADDITIONAL INFORMATION – GWINNETT COUNTY LEVEL 2 TRAFFIC IMPACT STUDY

The following sections have been included per Gwinnett County's Traffic Impact Study Guidelines (updated June 30, 2023) to meet the requirements of a Level 2 traffic study. Additional information regarding access management/spacing, intersection sight distance, or connectivity and circulation should be requested from the civil site engineer.

4.1 Future ADT Volumes

As shown in section 2 (Page 3), the roads within the study network with AADT volumes recorded by GDOT are SR 20 (Buford Drive) and Russell Road. The daily traffic volume on SR 20 (Buford Drive) in 2023 was 34,500 vehicles per day north of Russell Road. Based on the proposed site traffic generation/distribution, it is estimated that the AADT on SR 20 (Buford Drive) would be approximately 35,390 vehicles per day following the completion of the development in 2026. The AADT on Russell Road in 2023 was 8,220 vehicles per day west of Calvin Davis Drive, and it is estimated that this volume will increase to approximately 8,670 vehicles per day at this location following the completion of the development in 2026. Given how these estimated increases also factor in an assumed annual growth rate of 1% from 2024 to 2026, it can be concluded that the impact of the proposed site on the daily traffic for the roadways within the study network will be minimal.

4.2 Truck Volumes and Circulation

Since the proposed site will be residential, it is not expected to have any notable impact on truck volumes/circulation in the study network.

4.3 Summary of Existing Pedestrian and Bicycle Facilities and Connectivity

Pedestrian crosswalks exist at the signalized intersection of SR 20 (Buford Drive) and Russell Road. Sidewalks exist along all roadways within the vicinity of the site, except for the section of Calvin Davis Circle to the south of Russell Road. This study does not include any recommendations for new pedestrian and/or bicycle facilities within or near the study network. Any additional information regarding planned pedestrian and/or bicycle facilities within the proposed residential development itself should be requested from the civil site engineer.

4.4 Existing Transit Routes and Stops/Proposed Transit Routes

None of Gwinnett County's current transit routes or stops are located on near the SR 20 (Buford Drive) corridor that falls within the study network. There are no bus stops located within a 1-mile radius of the proposed development. No transit routes have been proposed to service this development in the future.

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4.5 Traffic Operation Analysis Requirements

A detailed description of the study methodology is available in section 3 on pages 4-5. A description of the site trip generation and distribution estimations is available on page 15.

4.6 Future Identified Projects (i.e., GCCTP, GDOT, SPLOST)

There are no planned/scheduled GDOT/Gwinnet County projects that will be completed within the study network or that will have any notable impact on the study network.

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5.0 EXISTING 2024 TRAFFIC ANALYSIS

5.1 Existing Traffic Volumes

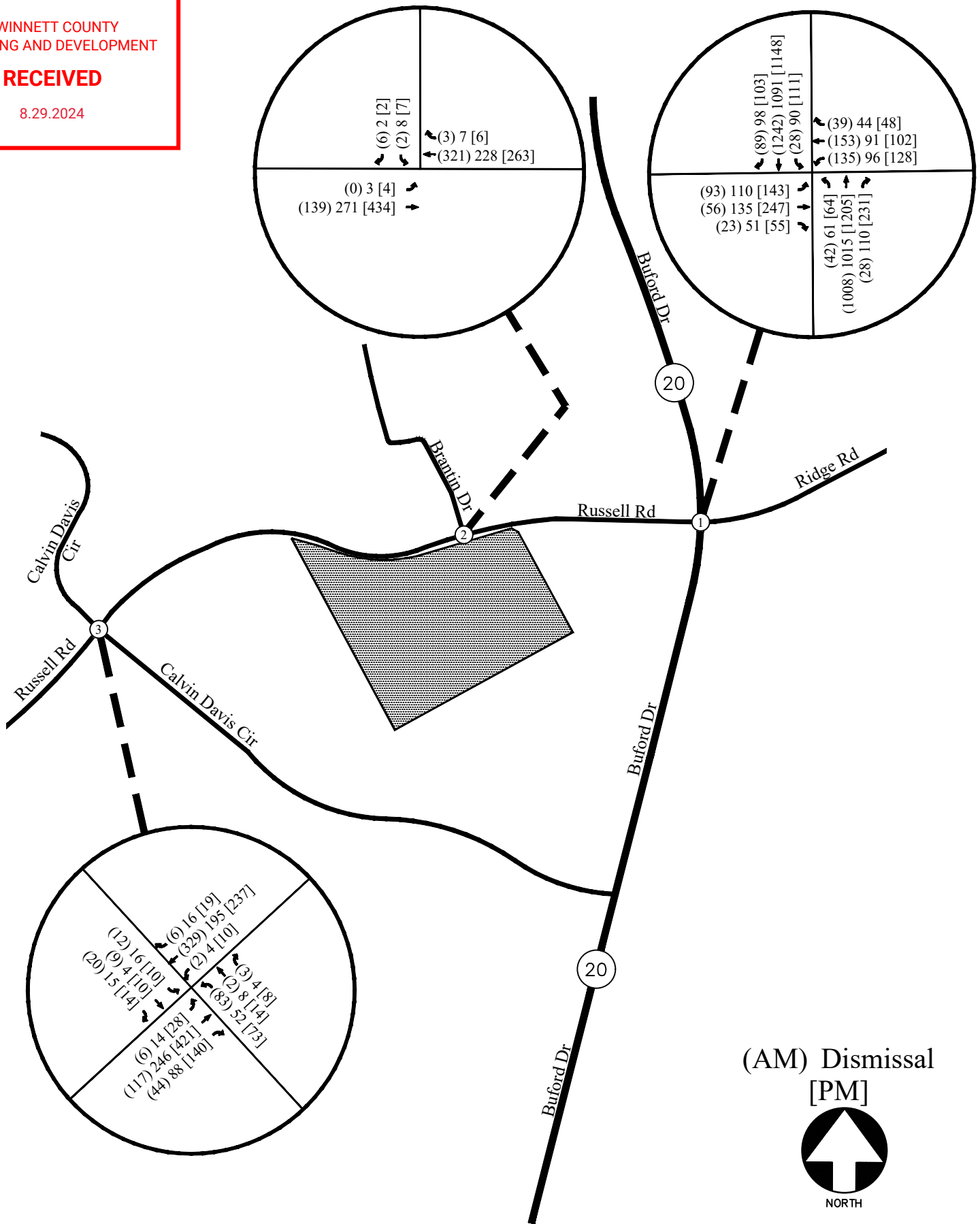
Existing traffic counts were obtained at the following study intersections:

1. SR 20 (Buford Drive) at Russell Road / Ridge Road
2. Russell Road at Brantin Drive
3. Russell Road at Calvin Davis Circle

Turning movement counts were collected on Wednesday, July 24, 2024. All turning movement counts were recorded during the AM and PM peak hours between 7:00 AM to 9:00 AM and 2:00 PM to 6:00 PM, respectively. The four consecutive 15-minute interval volumes that produced the highest volume at the intersections were then determined. These counted volumes make up the peak hour traffic volumes for the intersection and are shown in Figure 2. The existing AM and PM traffic volumes were increased by 10% (and the afternoon dismissal peak hour volumes were increased by 15%) to account for the fact that schools were not in session when the data was collected. These adjusted volumes are shown in Figure 3 and were used in the existing traffic operations analysis. The existing traffic control and lane geometry for the study intersections are shown in Figure 4.

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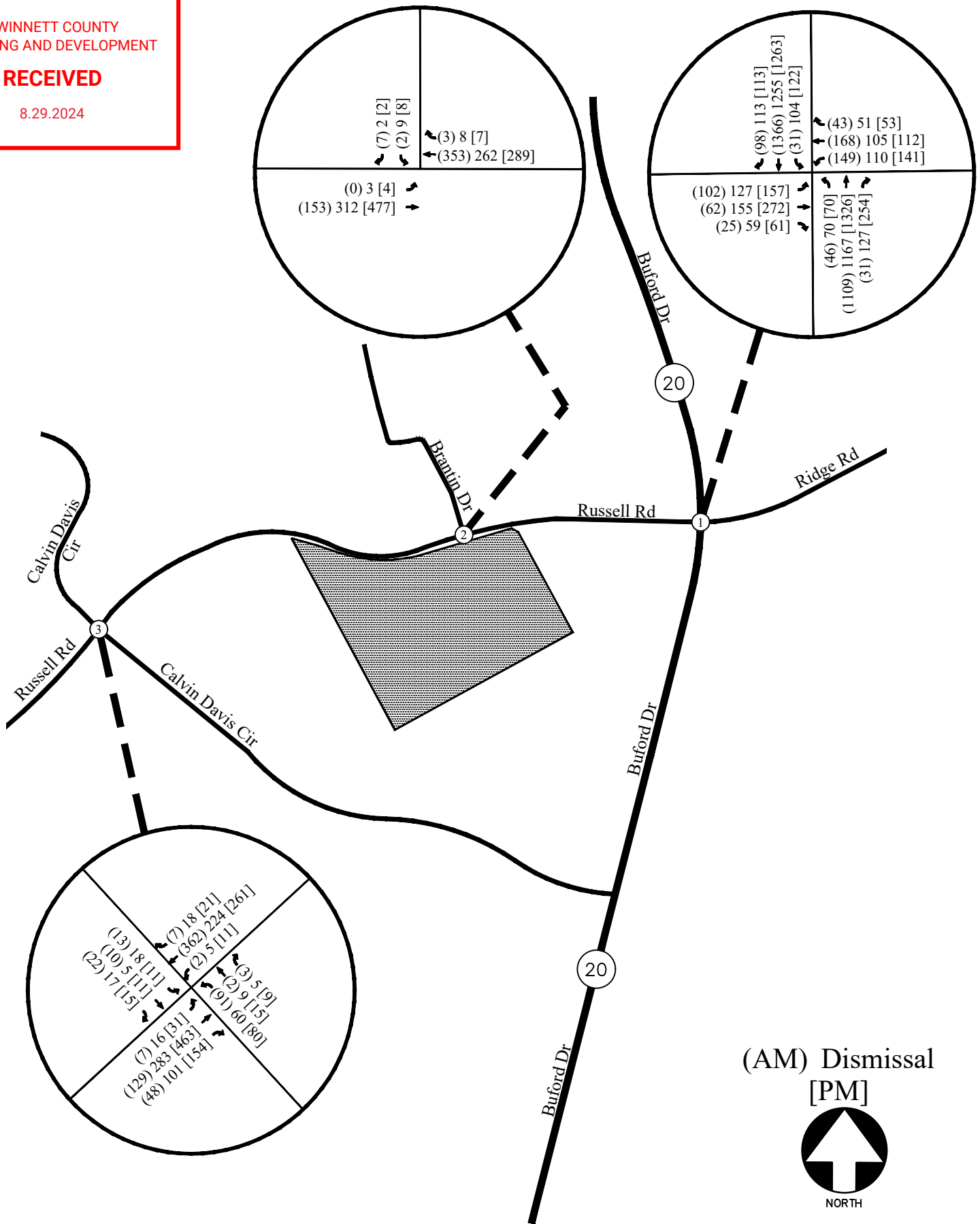


EXISTING WEEKDAY PEAK-HOUR VOLUMES
(WITHOUT SCHOOL TRAFFIC)

FIGURE 2
A&R Engineering Inc.

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

EXISTING WEEKDAY PEAK-HOUR VOLUMES
(ADJUSTED FOR SCHOOL TRAFFIC)

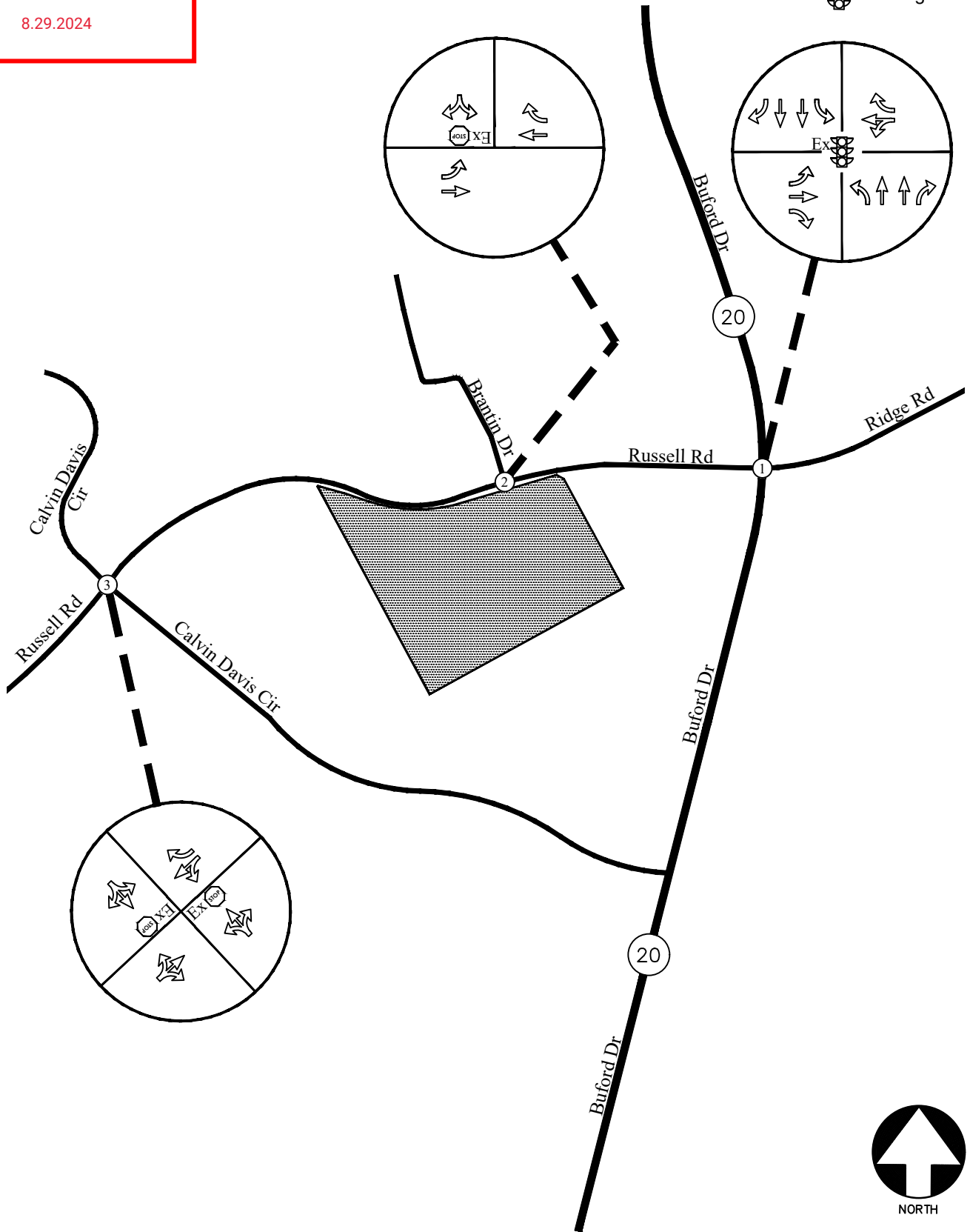
FIGURE 3
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LEGEND

- Ex  Existing Signed Approach
-  Existing Lane Geometry
- Ex  Existing Traffic Signal



EXISTING TRAFFIC CONTROL AND LANE GEOMETRY

FIGURE 4

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5.2 Existing Traffic Operations

Existing 2024 traffic operations were analyzed at the study intersections in accordance with the HCM methodology. The results of the analysis are shown in Table 3.

TABLE 3 – EXISTING INTERSECTION OPERATIONS				
Intersection	Traffic Control	LOS (Delay)		
		AM Peak	School Dismissal	PM Peak
1 <u>SR 20 (Buford Drive) @ Russell Road / Ridge Road</u> -Eastbound Approach -Westbound Approach -Northbound Approach -Southbound Approach	Signalized	<u>C (32.2)</u>	<u>C (29.3)</u>	<u>D (44.8)</u>
		E (64.3)	E (56.2)	E (68.7)
		E (64.9)	E (56.6)	E (74.9)
		C (23.1)	C (32.2)	D (39.6)
		C (27.5)	C (23.8)	D (36.6)
2 <u>Russell Road @ Brantin Drive</u> -Eastbound Left -Southbound Approach	Stop Controlled on SB Approach	A (8.2)	A (7.9)	A (7.9)
		B (11.2)	B (13.0)	B (14.9)
3 <u>Russell Road @ Calvin Davis Circle</u> -Eastbound Left -Westbound Left -Northbound Approach -Southbound Approach	Stop Controlled on NB and SB Approaches	A (8.2)	A (7.8)	A (8.0)
		A (7.7)	A (8.1)	A (9.0)
		C (20.0)	C (17.2)	E (38.0)
		B (13.7)	B (13.3)	B (19.1)

The results of the existing traffic operations analysis indicate that the signalized study intersection of SR 20 (Buford Drive) is operating at an overall level of service “D” or better in the AM, school dismissal, and PM peak hours. The stop-controlled approaches at the unsignalized study intersections are operating at a level of service “D” or better with peak hour traffic, except for the northbound (Calvin Davis Circle) approach at intersection 3, which will operate at a level of service “E” in the PM peak hour.

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6.0 PROPOSED DEVELOPMENT

The proposed residential development will be located at 598 Russell Road in Gwinnett County and will consist of 112 townhomes.



The development proposes one full access driveway on Russell Road.

A site plan is shown in Figure 5.

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6.1 Trip Generation

Trip generation estimates for the project were based on the rates and equations published in the 11th edition of the Institute of Transportation Engineers (ITE) Trip Generation report. This reference contains traffic volume count data collected at similar facilities nationwide. The trip generation was based on the ITE land use category 215 – *Single-Family Attached Housing*. The calculated total trip generation for the proposed development is shown in Table 4.

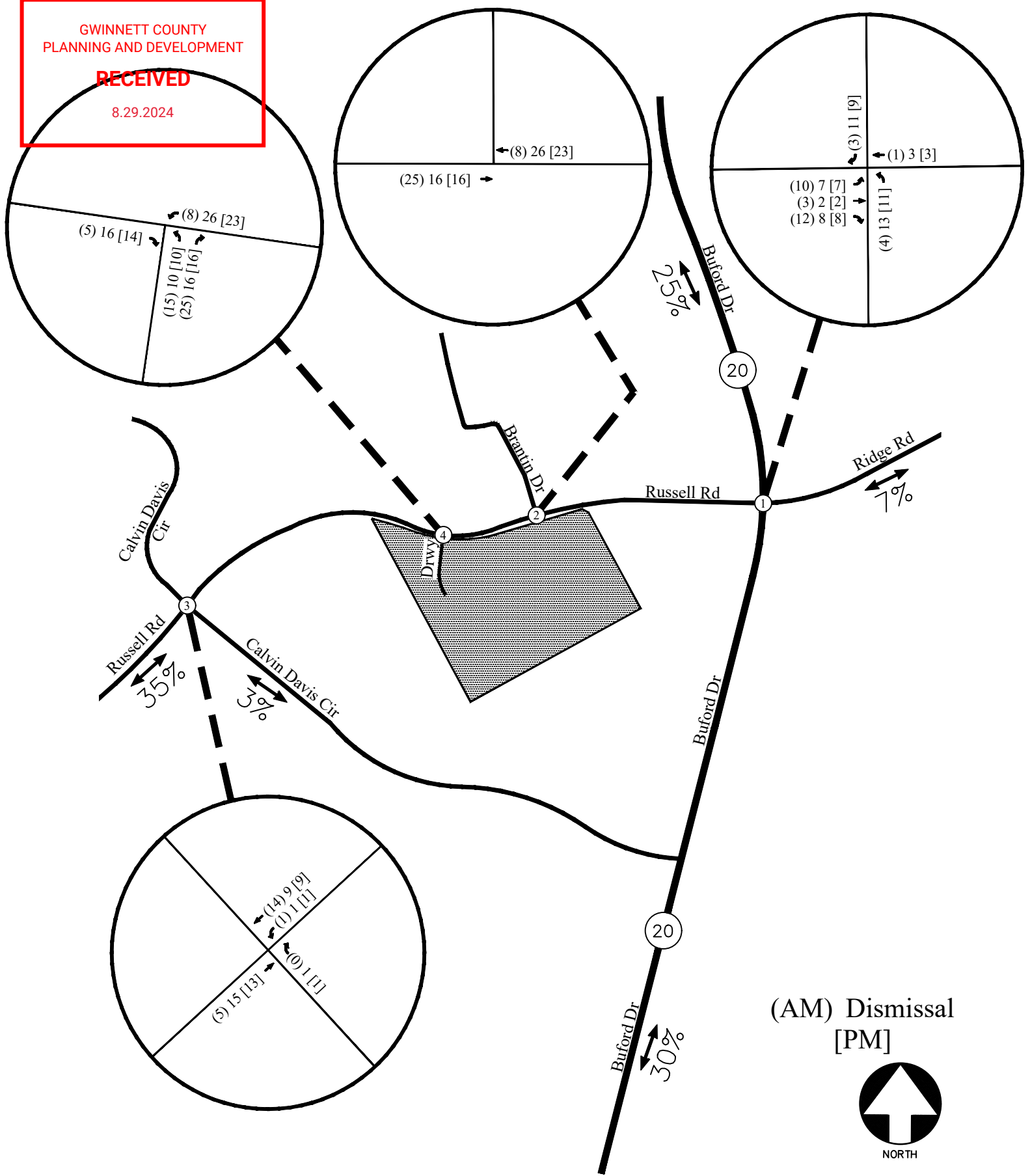
TABLE 4 – TRIP GENERATION											
Land Use	Size	AM Peak Hour			School Dismissal Peak Hour			PM Peak Hour			24 Hour
		Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Two-Way
ITE 215 – Single-Family Attached Housing	112 Units	13	40	53	42	26	68	37	26	63	803

6.2 Trip Distribution

The trip distribution describes how traffic arrives and departs from the site. An overall trip distribution was developed for the site based on a review of the existing travel patterns in the area and the locations of major roadways and highways that will serve the development. The site-generated peak hour traffic volumes, shown in Table 4, were assigned to the study area intersections based on this distribution. The outer-leg distribution and AM, school dismissal, and PM peak hour traffic volumes generated by the site are shown in Figure 6.

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(AM) Dismissal
[PM]



TRIP DISTRIBUTION AND NEW SITE-GENERATED
WEEKDAY PEAK HOUR VOLUMES

FIGURE 6
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7.0 FUTURE 2026 TRAFFIC ANALYSIS

The future 2026 traffic operations are analyzed for the “Build” and “No-Build” conditions.

7.1 Future “No-Build” Conditions

The “No-Build” (or background) conditions provide an assessment of how traffic will operate in the study horizon year without the study site being developed as proposed, with projected increases in through traffic volumes due to normal annual growth. The future “No-Build” volumes consist of the existing traffic volumes (Figure 2) plus increases due to the annual growth of through traffic.

7.1.1 Annual Traffic Growth

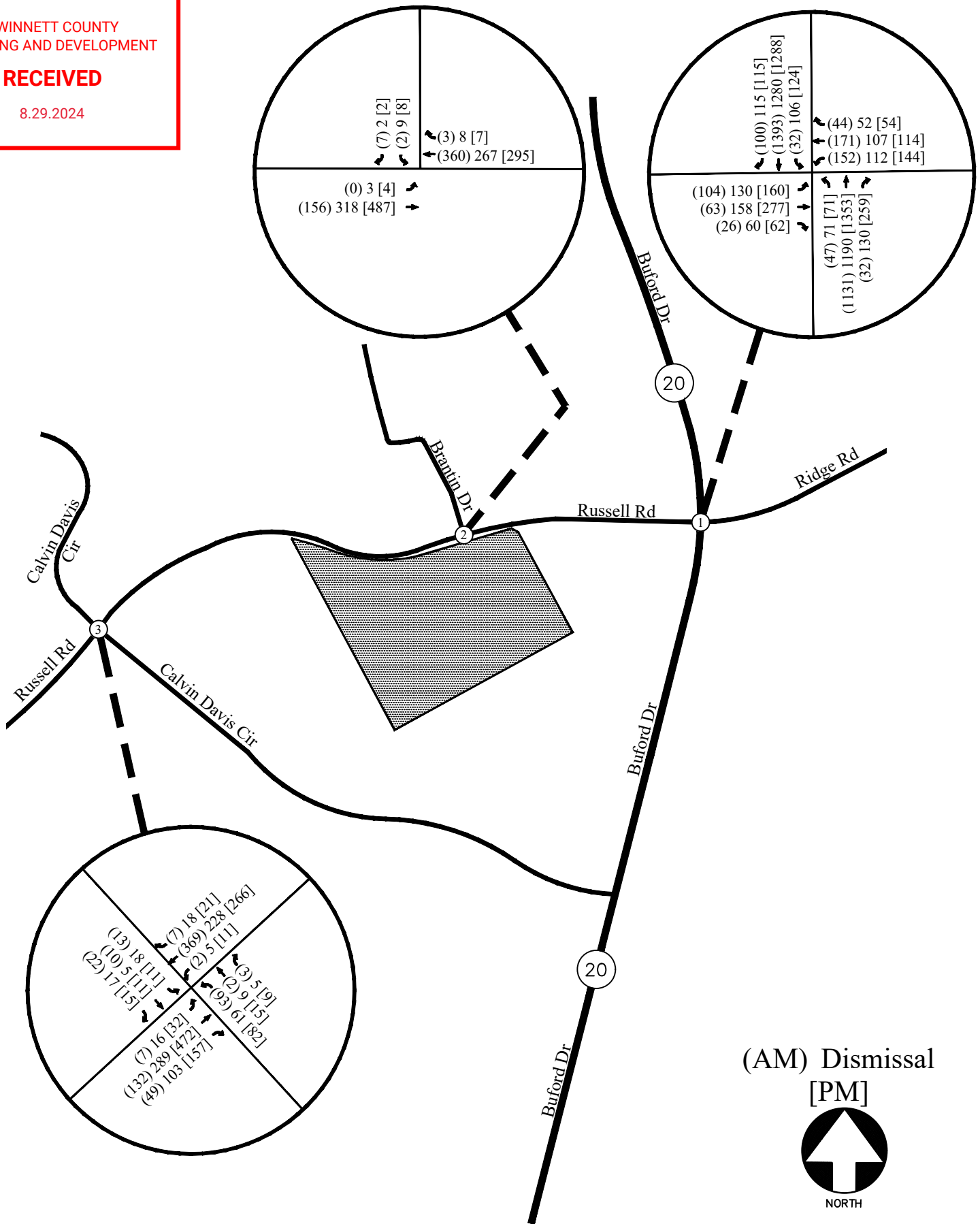
To evaluate future traffic operations in this area, a projection of normal traffic growth was applied to the existing volumes. The Georgia Department of Transportation recorded average daily traffic volumes at several locations in the vicinity of the site. Reviewing the growth over the last five years (2018-2019 & 2021-2023) revealed a traffic volume increase of approximately 1% in the area. This growth factor was applied to the existing traffic volumes between collector and arterial roadways to estimate the future year traffic volumes prior to the addition of site-generated traffic. The resulting future “No-Build” volumes on the roadway are shown in Figure 7.

7.2 Future “Build” Conditions

The “Build” or development conditions include the estimated background traffic from the “No-Build” conditions plus the added traffic from the proposed development. To evaluate future traffic operations in this area, the additional traffic volumes from the site (Figure 6) were added to base traffic volumes (Figure 7) to calculate the future traffic volumes after the construction of the development. These total future “Build” traffic volumes are shown in Figure 8.

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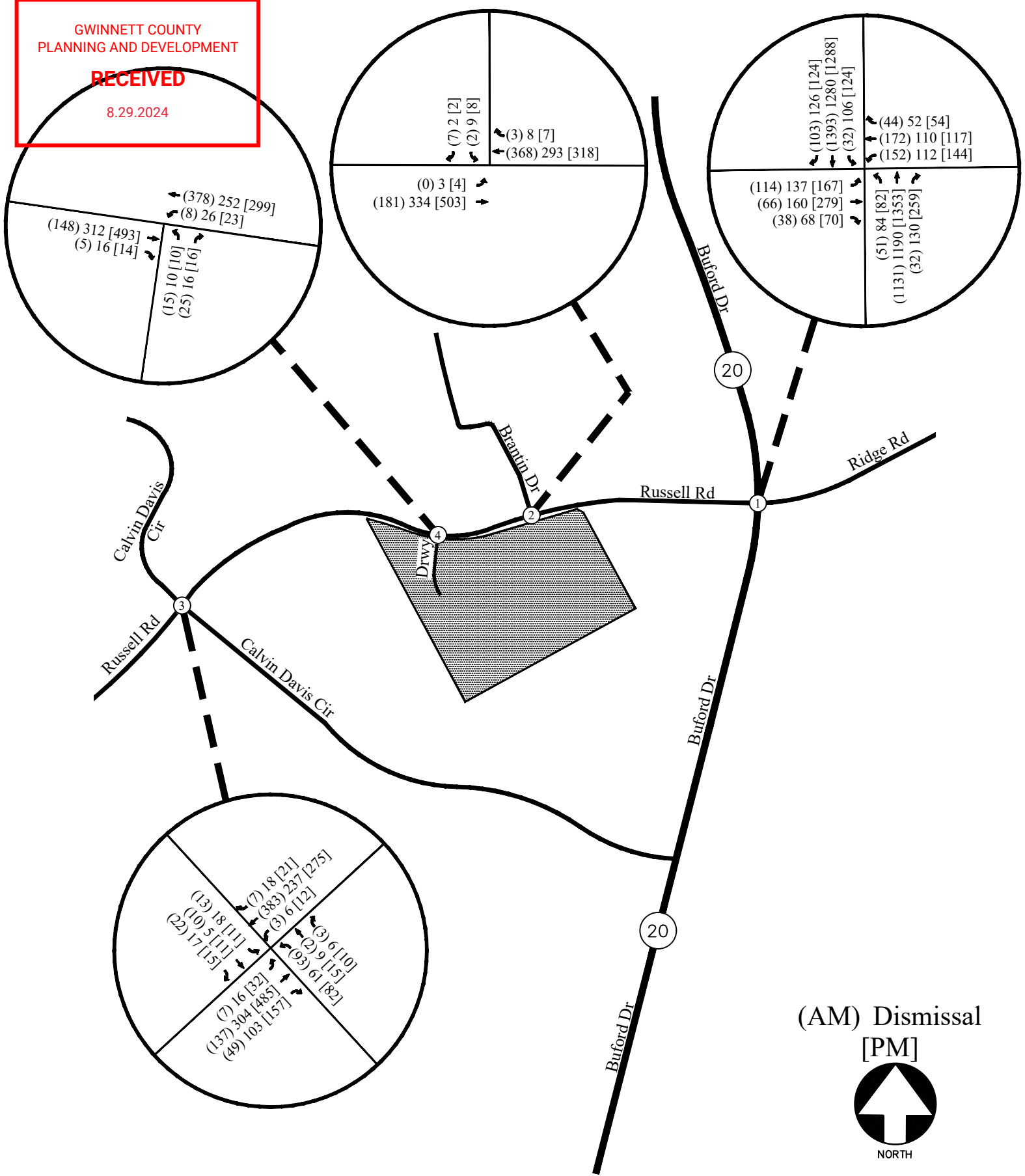
FUTURE (NO-BUILD) WEEKDAY PEAK HOUR VOLUMES

FIGURE 7

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(AM) Dismissal

[PM]



FUTURE (BUILD) WEEKDAY PEAK HOUR VOLUMES

FIGURE 8

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7.3 Auxiliary Lane Analysis

Included below are analyses for right turn lanes for the site driveway. The analyses below are based off the trip distribution included in Section 5.2. According to the trip distribution, the overall 24-hour two-way volume for traffic entering and exiting the site are 803 vehicles.

7.3.1 Left Turn Lane Analysis

According to Gwinnett County Criteria and Guidelines for Left Turn Lanes, left turn lanes must be provided at each subdivision street that accesses a minor/major collector street or major thoroughfare if the characteristics of the major street and site density exceed certain thresholds. Per Gwinnett County left turn lane standards (Table 1 – Residential Developments), the minimum threshold for a left turn lane on a 40-mph road with an ADT above 6,000 is 65 lots/units. Since the proposed development will have 112 townhome units and will therefore exceed the threshold, a left turn lane is warranted at the site driveway.

7.3.2 Deceleration Turn Lane Analysis

According to Gwinnett County Unified Development Ordinance (UDO) Standards, a right turn deceleration lane shall be required at each subdivision street entrance that is provided street access to a Minor/Major Collector Street or Major Thoroughfare (900-30.2). Since Russell Road is classified by Gwinnett County as a Major Collector Street, a right turn lane is warranted at the site driveway.

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7.4 Future Traffic Operations

The future “No-Build” and “Build” traffic operations were analyzed using the volumes in Figures 7 and 8, respectively. The results of the future traffic operations analyses are shown below in Table 5. Recommendations on traffic control and lane geometry are shown in Figure 9.







TABLE 5 – FUTURE INTERSECTION OPERATIONS							
Intersection		Future Condition: LOS (Delay)					
		NO-BUILD			BUILD		
		AM	Dismissal	PM	AM	Dismissal	PM
1	<u>SR 20 (Buford Drive) @ Russell Road / Ridge Road</u>	<u>C (33.4)</u>	<u>C (30.3)</u>	<u>D (47.4)</u>	<u>C (34.6)</u>	<u>C (30.9)</u>	<u>D (48.2)</u>
	-Eastbound Approach	E (65.7)	E (57.5)	E (70.7)	E (65.9)	E (58.2)	E (71.0)
	-Westbound Approach	E (67.3)	E (58.8)	E (77.4)	E (68.9)	E (59.8)	E (78.2)
	-Northbound Approach	C (23.9)	C (24.0)	D (42.4)	C (24.6)	C (24.4)	D (43.1)
	-Southbound Approach	C (28.6)	C (24.7)	D (39.0)	C (29.7)	C (25.1)	D (40.1)
2	<u>Russell Road @ Brantin Drive</u>						
	-Eastbound Left	A (8.2)	A (7.9)	A (7.9)	A (8.2)	A (8.0)	A (8.0)
	-Southbound Approach	B (11.3)	B (13.1)	C (15.1)	B (11.5)	B (13.6)	C (15.7)
3	<u>Russell Road @ Calvin Davis Circle</u>						
	-Eastbound Left	A (8.3)	A (7.8)	A (8.0)	A (8.3)	A (7.8)	A (8.0)
	-Westbound Left	A (7.7)	A (8.2)	A (9.0)	A (7.7)	A (8.2)	A (9.1)
	-Northbound Approach	C (20.7)	C (17.6)	E (41.2)	C (21.7)	C (18.2)	E (44.5)
	-Southbound Approach	B (13.9)	B (13.5)	C (19.5)	B (14.2)	B (13.8)	C (20.2)
4	<u>Russell Road @ Site Driveway</u>						
	-Westbound Left	-	-	-	A (7.6)	A (8.1)	A (8.6)
	-Northbound Approach	-	-	-	B (10.8)	B (11.8)	B (14.4)

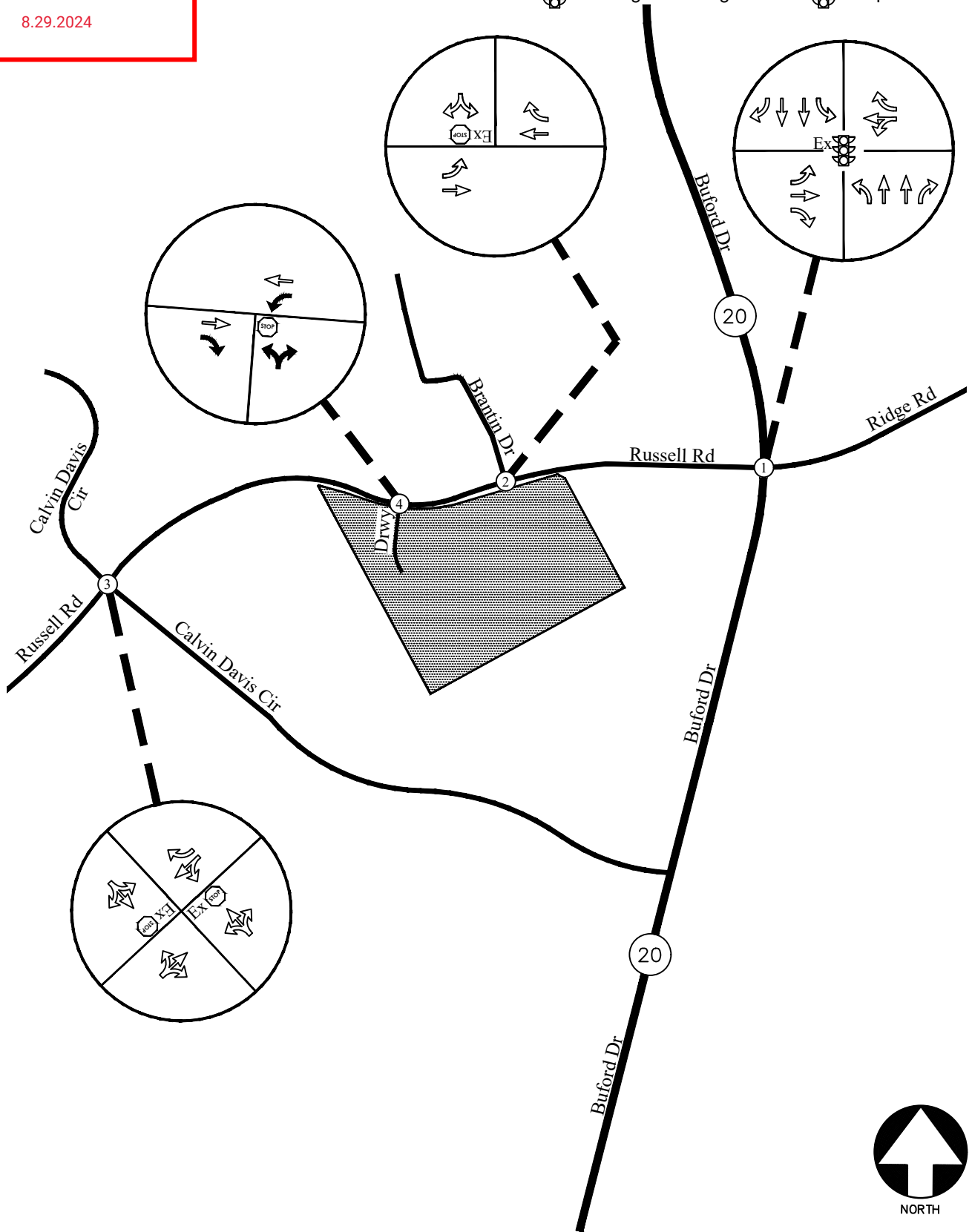
The results of the “No Build” and “Build” traffic operations analysis indicate that the signalized study intersection of SR 20 (Buford Drive) will continue to operate at an overall level of service “D” or better in the AM, school dismissal, and PM peak hours. The stop-controlled approaches at the unsignalized study intersections will operate at a level of service “D” or better with peak hour traffic, except for the northbound (Calvin Davis Circle) approach at intersection 3, which will operate at a level of service “E” in the PM peak hour under both “No-Build” and “Build” conditions. It is not unusual for minor side streets that are stop-controlled to experience higher delays due to the time gap required for vehicles to make turning movements on a busy collector roadway.

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LEGEND

- | | | | |
|--|--------------------------|---|--------------------------|
| Ex  | Existing Signed Approach |  | Proposed Signed Approach |
|  | Existing Lane Geometry |  | Proposed Lane Geometry |
| Ex  | Existing Traffic Signal |  | Proposed Traffic Signal |



FUTURE TRAFFIC CONTROL AND LANE GEOMETRY

FIGURE 9

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8.0 CONCLUSIONS AND RECOMMENDATIONS

Traffic impacts were evaluated for the proposed residential development that will be located at 598 Russell Road in Gwinnett County, Georgia. The development will consist of 112 townhomes and proposes one full access driveway on Russell Road.

Existing and future operations after completion of the project were analyzed at the intersections of:

1. SR 20 (Buford Drive) at Russell Road / Ridge Road
2. Russell Road at Brantin Drive
3. Russell Road at Calvin Davis Circle
4. Russell Road at Site Driveway

The analysis included the evaluation of future operations for “No-Build” and “Build” conditions, with the differences between “No-Build” and “Build” accounting for increases in traffic due to the proposed development. The results of the “No Build” and “Build” traffic operations analysis indicate that the signalized study intersection of SR 20 (Buford Drive) will continue to operate at an overall level of service “D” or better in the AM, school dismissal, and PM peak hours. The stop-controlled approaches at the unsignalized study intersections will operate at a level of service “D” or better with peak hour traffic, except for the northbound (Calvin Davis Circle) approach at intersection 3, which will operate at a level of service “E” in the PM peak hour under both “No-Build” and “Build” conditions. It is not unusual for minor side streets that are stop-controlled to experience higher delays due to the time gap required for vehicles to make turning movements on a busy collector roadway. Based on the analysis, the proposed development will have minimal impact on traffic operations in the study network.

8.1 Recommendations for Site Access Configuration

The following access configuration is recommended for the proposed site driveway intersection:

- Site Driveway: Full Access Driveway on Russell Road
 - One entering lane and one exiting lane
 - Stop-sign controlled on the driveway approach with Russell Road remaining free flow
 - A right turn lane and a left turn lane for entering traffic
 - Provide/confirm adequate sight distance per AASHTO standards